

STATE OF MAINE
STATE HIGHWAY COMMISSION

PLAN AND PROFILE
STATE HIGHWAY "I"
CLINTON
KENNEBEC COUNTY

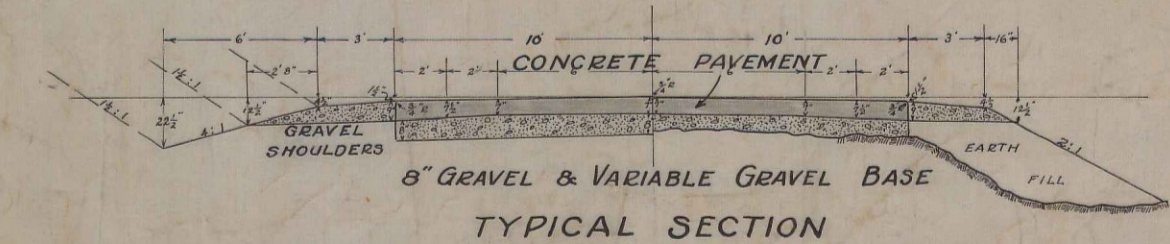
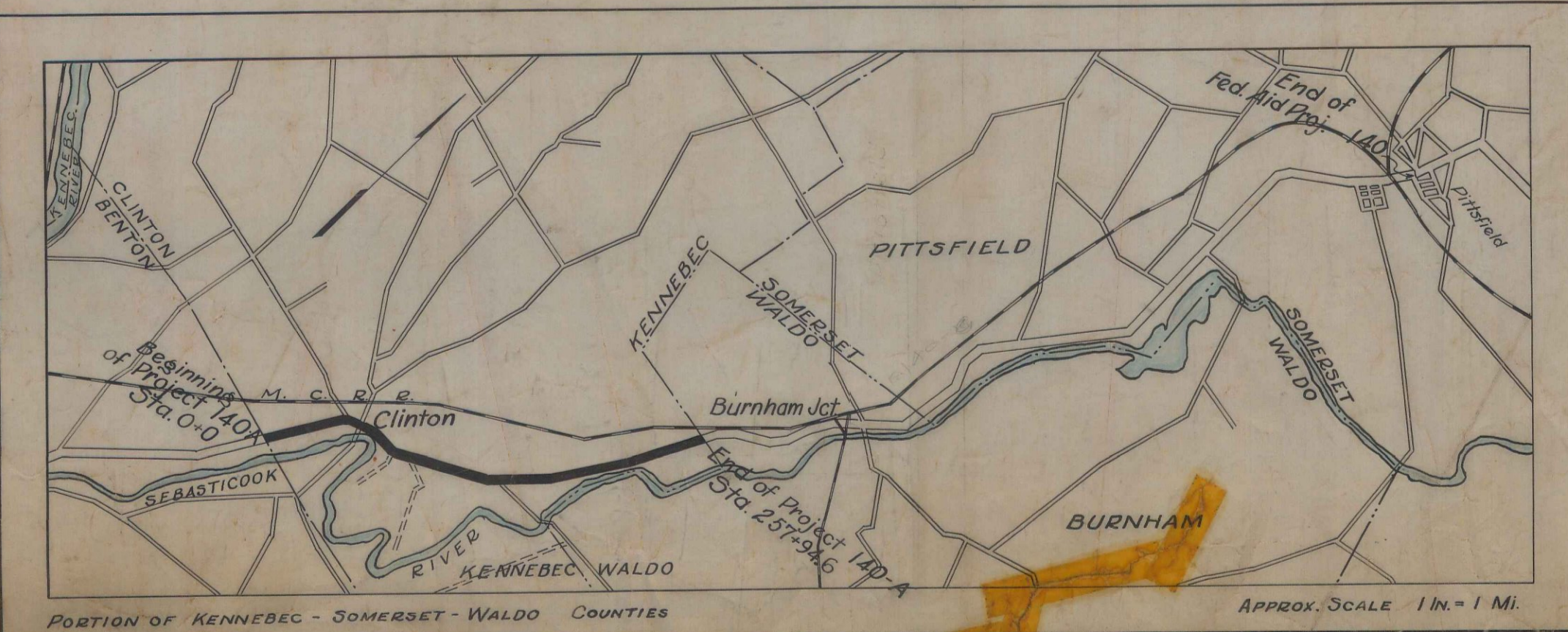
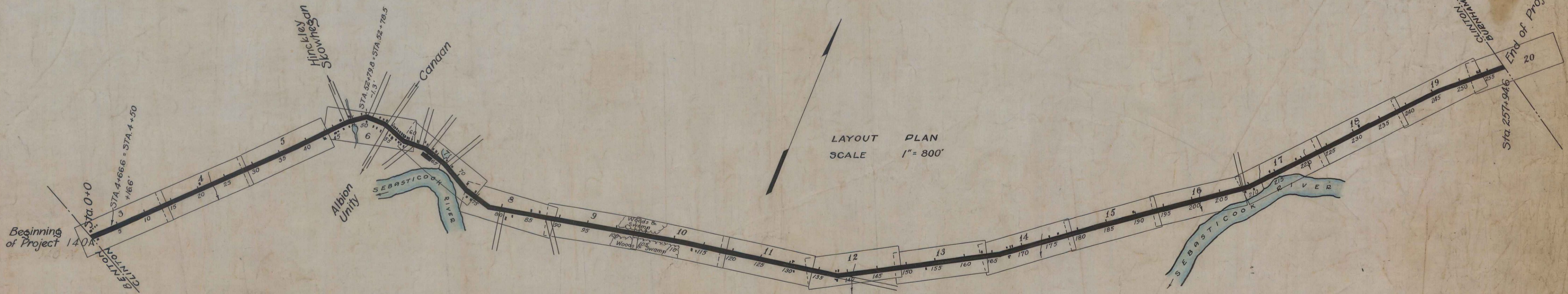
FEDERAL AID PROJECT NO. 140 A

TOTAL LENGTH 4.888 MILES

SCALES { PLAN 1" = 50 FT.
 { PROFILE { HOR. 1" = 50 FT.
 { VER. 1" = 5 FT.
 { CROSS SECTIONS 1" = 5 FT.

CONVENTIONAL SIGNS	
STATE OR NATIONAL LINE	-----
COUNTY LINE	-----
TOWN LINE	-----
UNFENCED PROPERTY	-----
FENCE	-----
RIGHT OF WAY LINE	-----
TRAVELED WAY	-----
RAILROAD	-----
RETAINING WALL	-----
SURVEY LINE	-----
CULVERT	-----
DROP INLET	-----
TROLLEY POLE	-----
POWER POLE	-----
TEL. POLE	-----
MARSH	-----
TREES	-----
STONE WALL	-----

INDEX OF SHEETS		
SHEET No. 1	TITLE PAGE	STA. 0 - 257+94.6
SHEET No. 2-3	TYPICAL SECTIONS	
SHEET No. 4-21	PLAN AND PROFILE	STA. 0 - 257+94.6
SHEET No. 22-41	CROSS-SECTIONS	STA. 0 - 257+94.6
SHEET No.	BRIDGES	STA.
SHEET No.	SPECIAL DETAILS	

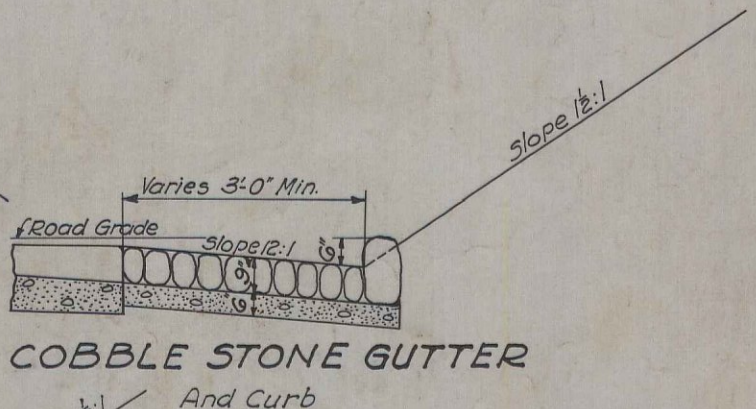
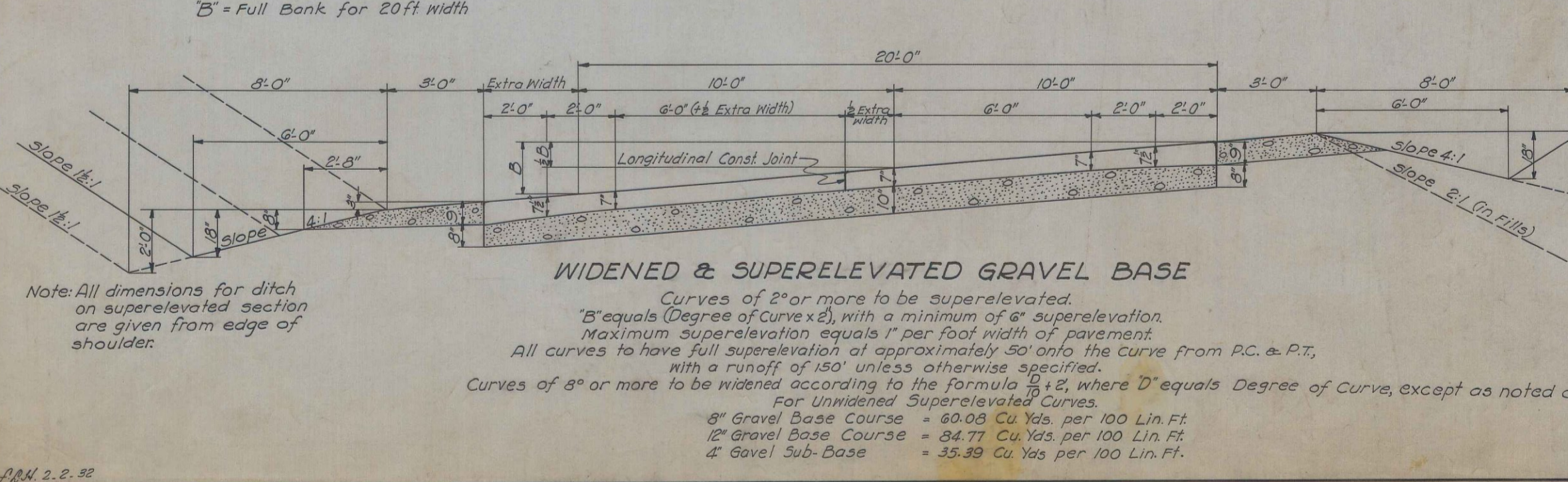
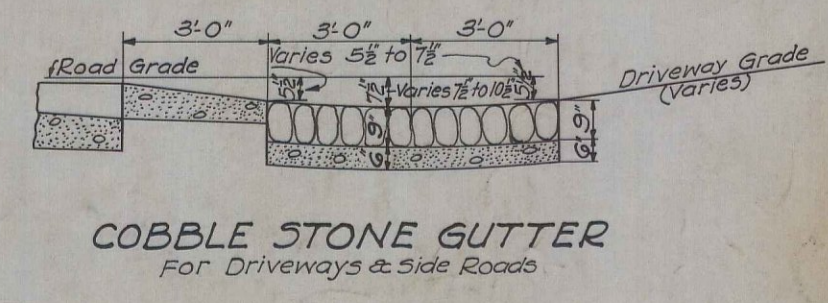
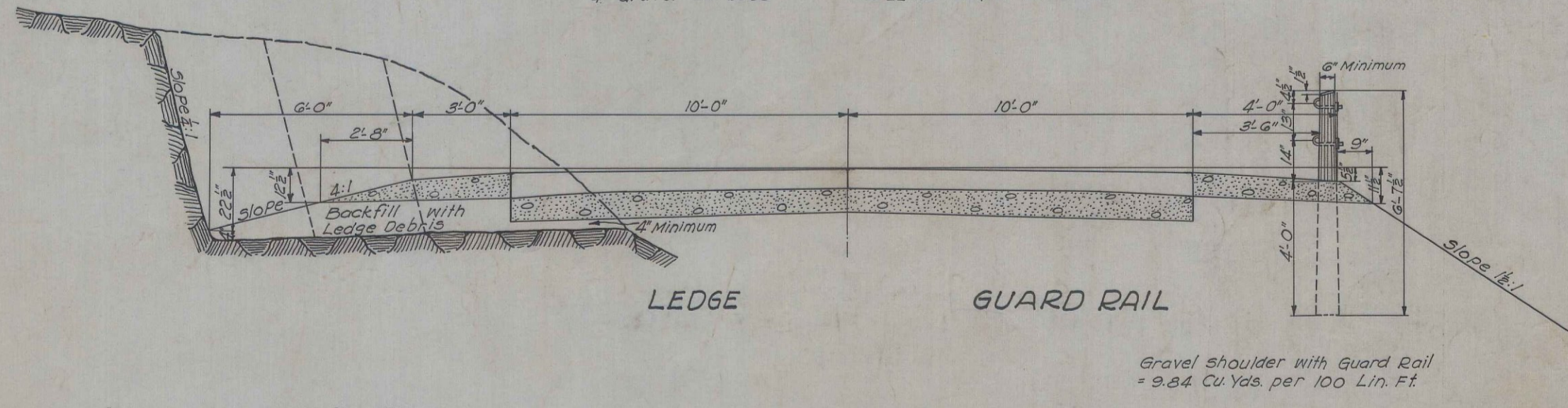
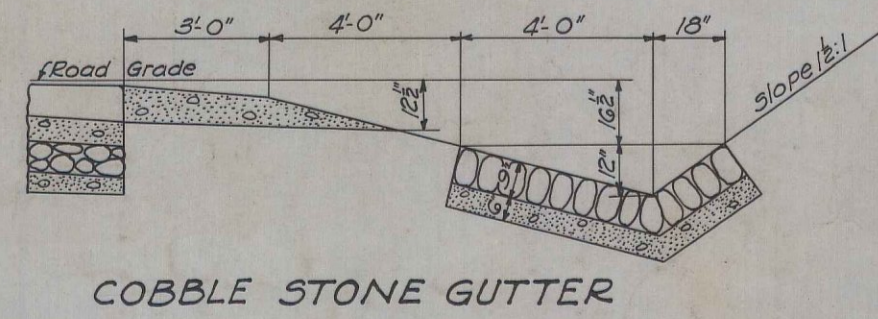
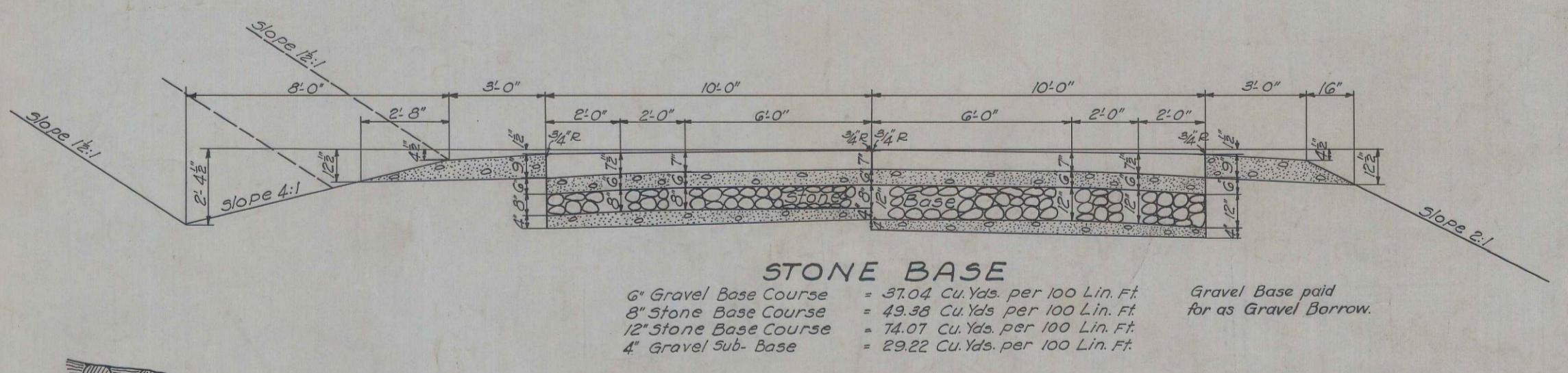
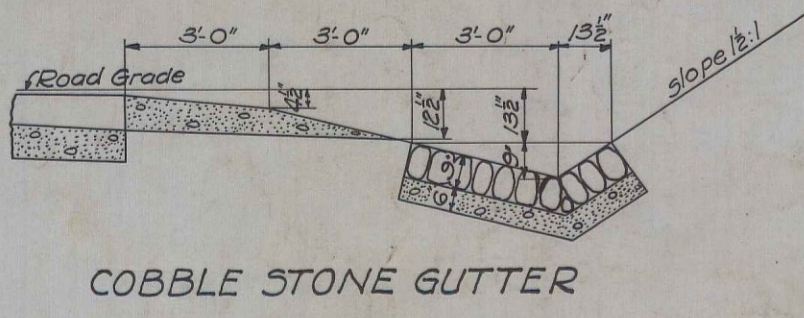
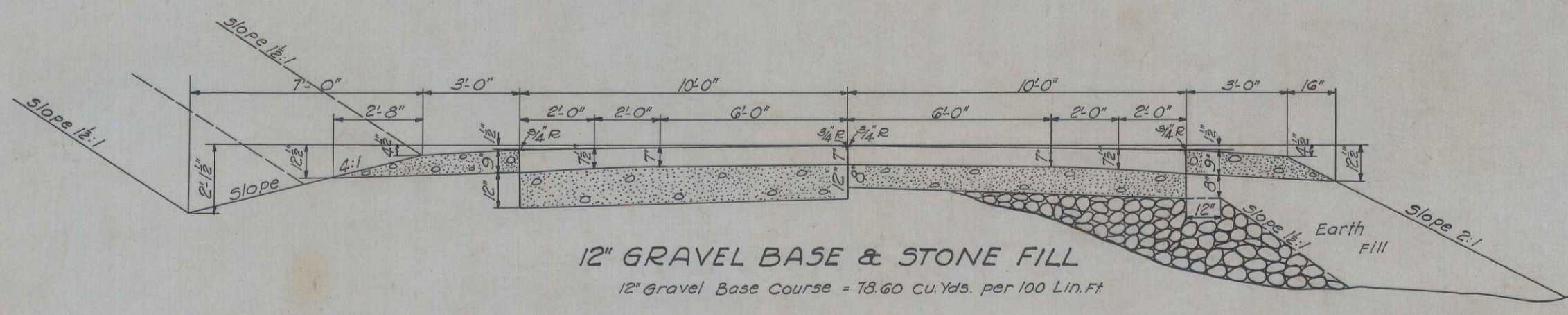
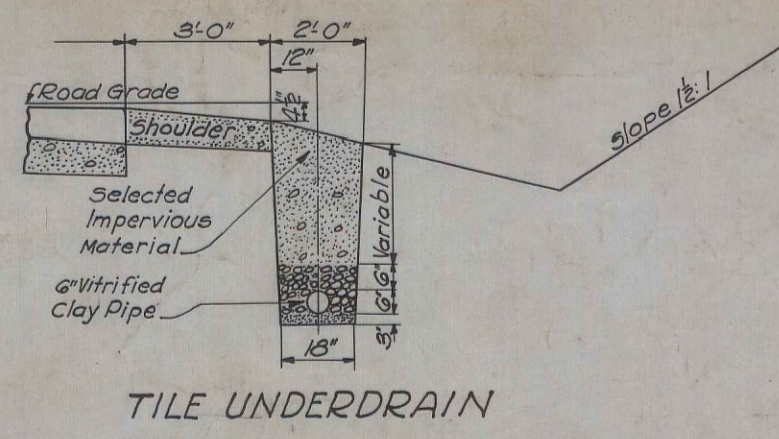
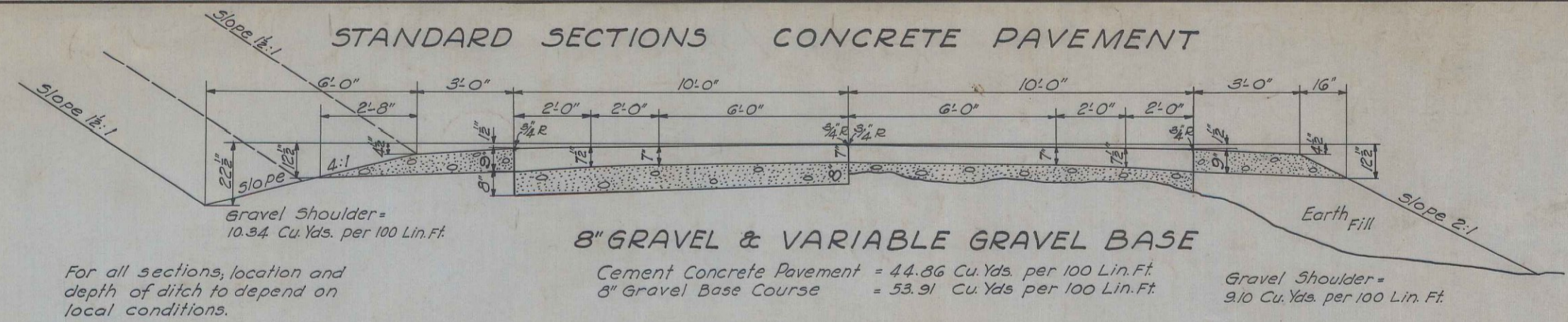


Note: All work contemplated under this contract to be covered by and in conformity with - The Specifications as revised 1932 except as modified on this plan.

APPROVED:
MAINE STATE HIGHWAY COMMISSION
Francis A. Peabody
CHAIRMAN
Edmund C. Hammond
William J. Bennett
CHIEF ENGINEER
Richard T. Barnes
CHIEF ENGINEER

APPROVED:
U. S. BUREAU OF PUBLIC ROADS
[Signature]
DISTRICT ENGINEER
[Signature]
CHIEF ENGINEER
[Signature]
DIRECTOR

STANDARD SECTIONS CONCRETE PAVEMENT



ESTIMATED QUANTITIES

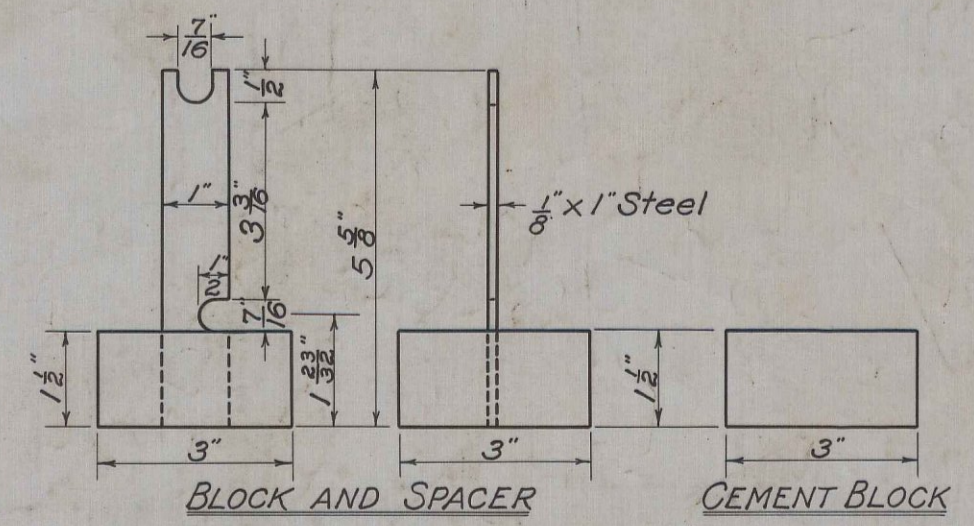
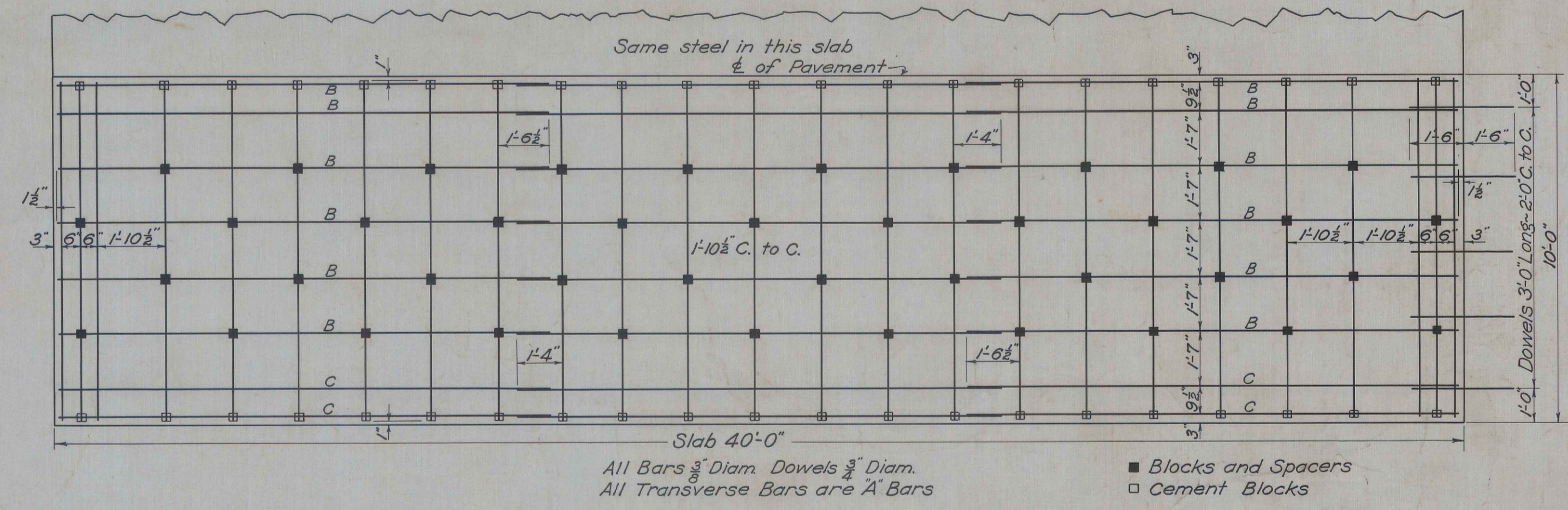
Item	Description	Quantity
12A	Earth Excavation	15,677 c.y.
12B	Rock Excavation	2,056 c.y.
12C	Trees Removed	6 Each
13	Excavation for Structures	1,955 c.y.
	Rock Excav. in Excava. for Structure	48 c.y.
16	Stone Fill	
17A	Gravel Borrow	36,868 c.y.
24	Stone Base Course	450 c.y.
33	Cement Concrete Pavement	11,612 c.y.
34A	Reinf. Steel for Concrete Pavement	618,363 Lbs
37A	Class "A" Concrete	76.1 c.y.
37B	Class "B" Concrete	254.2 c.y.
38	Steel Reinf. for Concrete Structures	9,332 Lbs
40	Cement Rubble Masonry	4,176 c.y.
42A	Laying 12" C.M.P.	582 L.F.
42C	" 18" C.M.P.	330 L.F.
44A	" 15" Conc. Pipe	358 L.F.
44C	" 18" Conc. Pipe	376 L.F.
44D	" 24" Conc. Pipe	120 L.F.
52A	Wire Cable Guard Rail	6,239 L.F.
52B	Anchorage for Wire Cable Guard Rail	71 Each
44A	Laying 12" R.C.P.	80 L.F.
45C	Laying 12" Vit. Clay Pipe	460 L.F.
45D	" 15" " " "	580 " "
46A	Drop Inlets	3 Each
46B	Catch Basins	6 " "
53	Bituminous Treatment	1,980 Galons

CULVERTS

Station	Kind	Size	Length	Remarks
13+96	R.C.P.	18"	36'	
21+98	Conc. Box	3'x3'	40'	
42+97	R.C.P.	15"	36'	
45+36	"	"	32'	
62+70	"	"	79'	
70+0	R.C.P.	18"	64'	
73+35	"	56"	56'	
75+85	"	18"	36'	
80+30	"	18"	40'	
97+13	"	15"	40'	
104+19	"	18"	36'	
123+22	"	18"	36'	
135+72	Conc. Box	3'x3'	46'	
140+36	"	4'x3'	46'	Extended 5'L - 6'R
165+46	R.C.P.	24"	40'	
172+64	"	24"	36'	
179+04	"	18"	40'	
189+81	"	18"	40'	
193+90	"	18"	48'	
200+72	Conc. Box	9'x6'	36'	
207+81	R.C.P.	15"	36'	
212+06	Conc. Box	3'x3'	36'	12'L - 11'R
215+68	R.C.P.	15"	36'	" 7'L - 8'R
220+61	Conc. Box	5'x3'	32'	" 6'L 6'R
231+21	R.C.P.	15"	32'	
235+77	Conc. Box	3'x2'	36'	
239+34	R.C.P.	24"	36'	
257+16	"	15"	33'	
12+10	"	15"	34'	
47+0	"	12"	80'	
61+50 L	12'x24'	74±L	18'x16'	
81+48 L	12'x32'	76±L	18'	
121+50 L	12'	81±50±R	120	
		6'	83±R	120
		14'	83±L	16
24±L	20	83±L	4	
32±L	36	83±68 L	30	
41±L	20	83±50 R	24	
87±L	16	86±0 L	20	
88±R	28	173±L	88	
141±R	18		8	
152±L	18		22	
	R	20	190±R	17
184±50 R	12		198±L	20
190±L	20			
190±L	20			
211±L	14			
212±L	20			
219±L	20			
219±L	20			
222±R	24			
235±L	14			
250±L	20			
250±L	24			
61±L	18			
	20			
	L	20		

NOTE: ALL GRAVEL SHOULDERS - GRAVEL BASE & GRAVEL SUB-BASE TO BE PAID FOR AS GRAVEL BORROW.

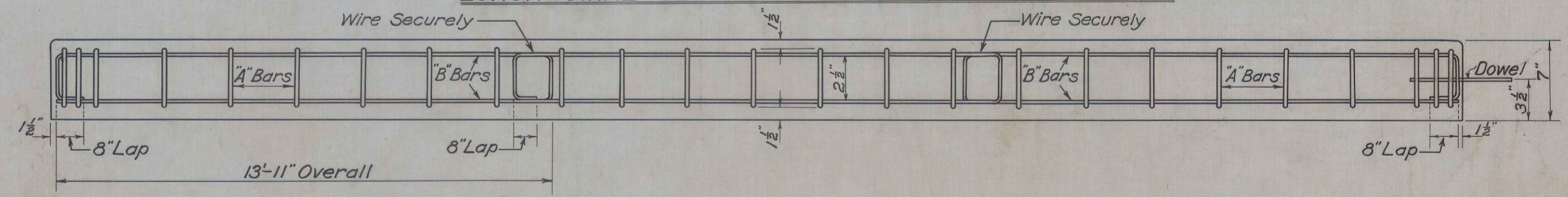
PLAN OF PAVEMENT REINFORCEMENT



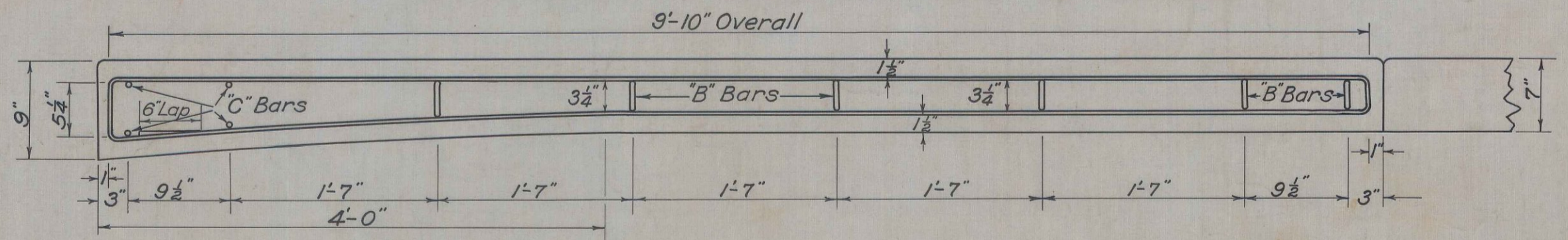
Blocks to be made of one part Portland Cement and two parts of approved sand and cured under cover for seven days.

Other types of steel supports may be used if approved by the Engineer.

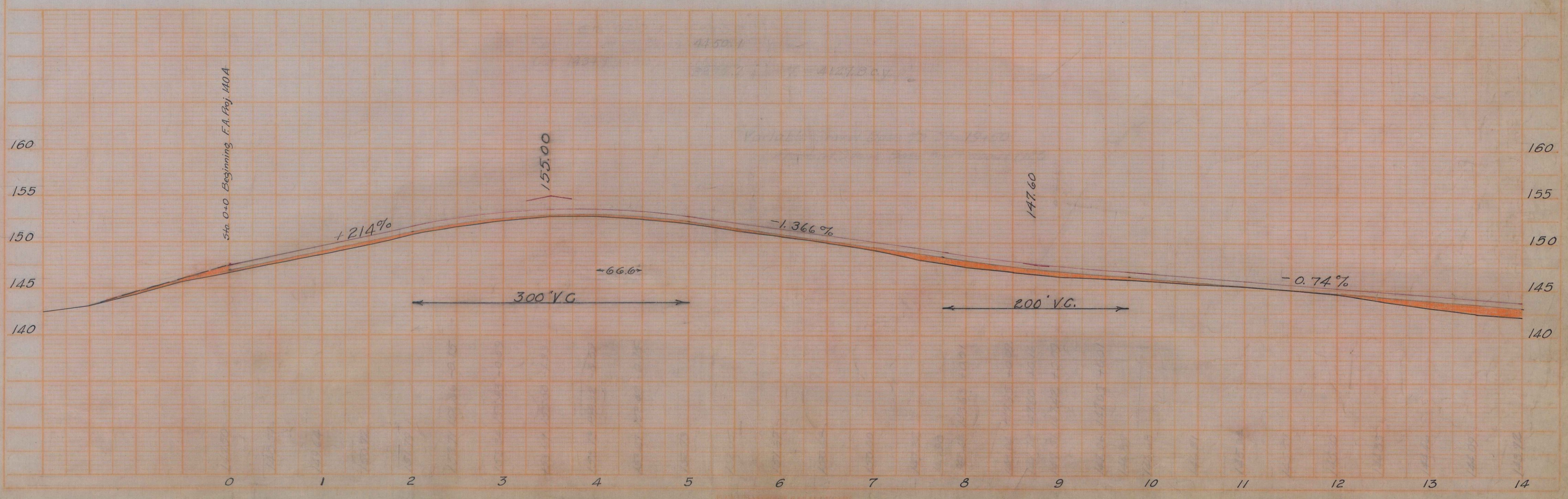
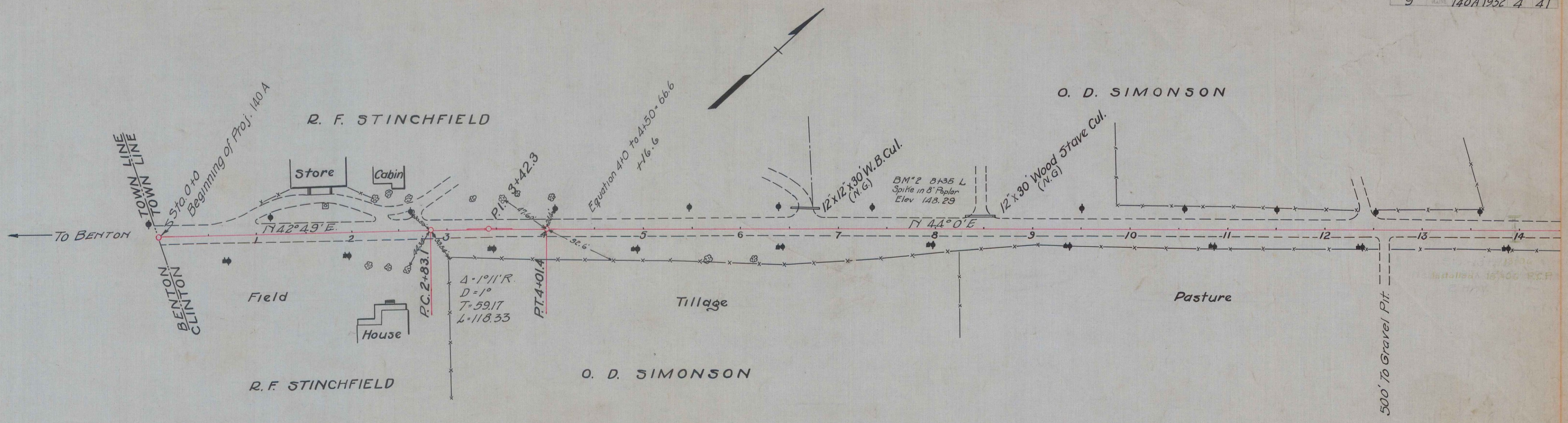
LONGITUDINAL SECTION SHOWING DETAIL OF "B" BARS

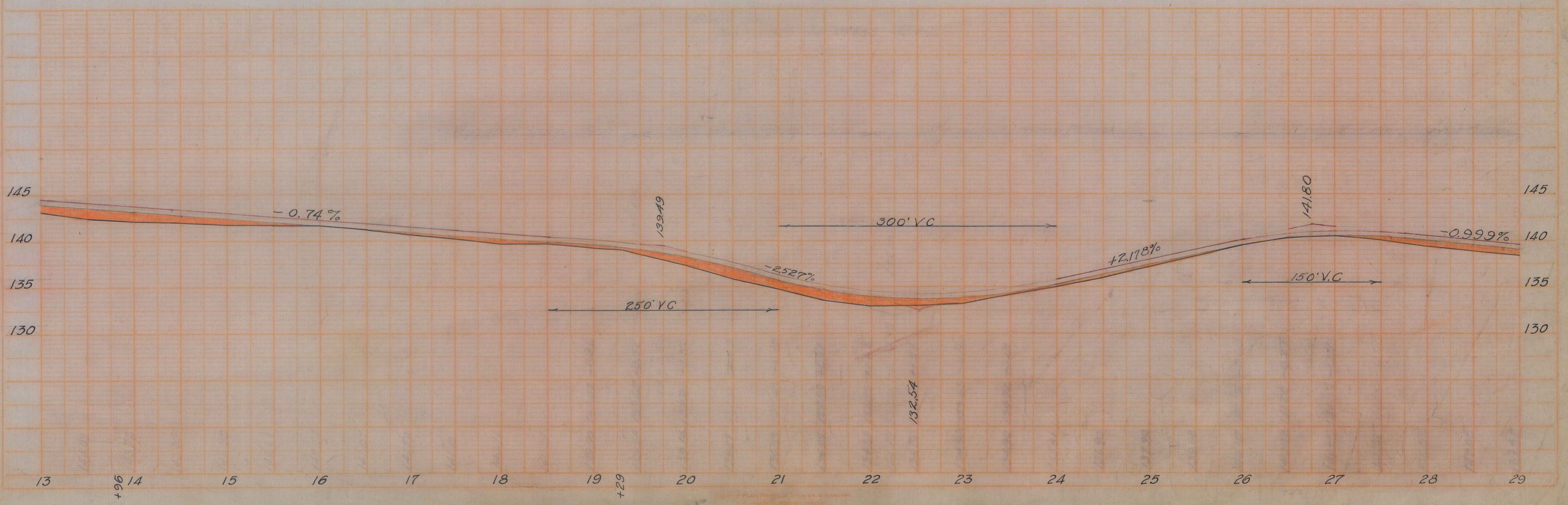
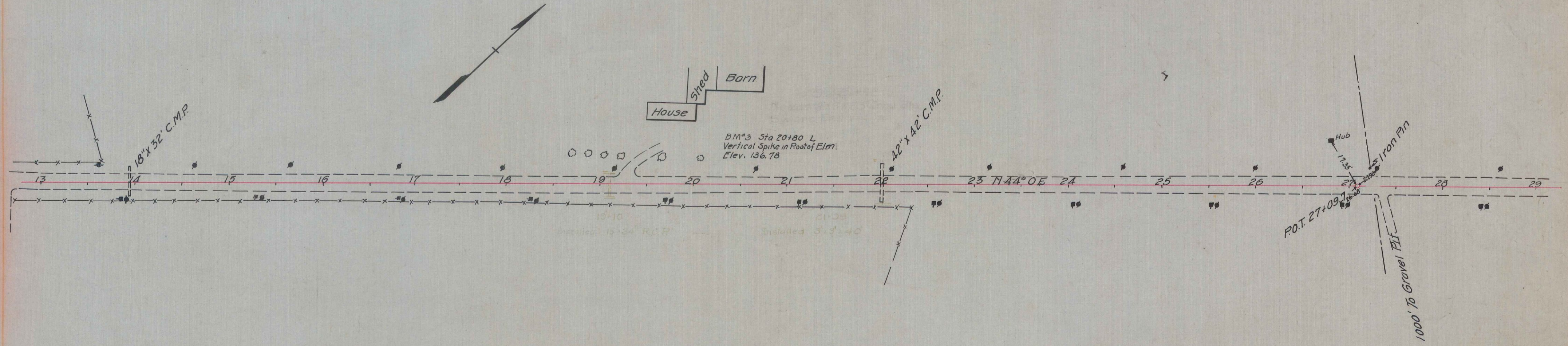


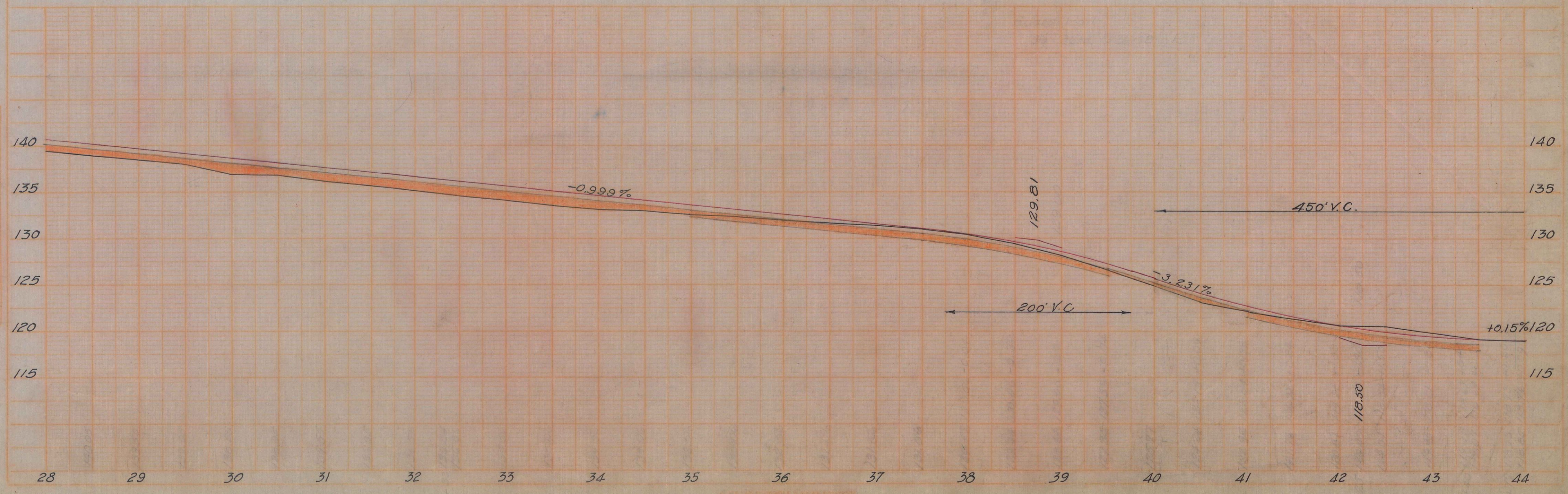
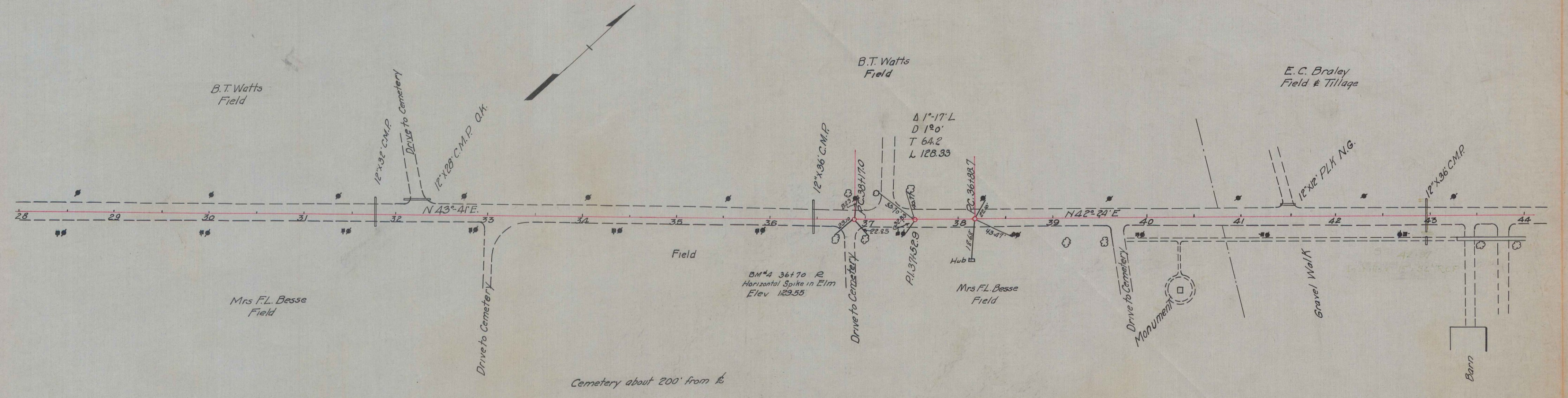
TRANSVERSE SECTION SHOWING DETAIL OF "A" BARS



REINFORCING DATA	
Bars	3/8" diam. plain steel - 0.376 lb. per lin. ft.
Dowels	3/4" " " - 1.502 " " " "
Dowels	3'-0" Long 1/2 encased in tube.
"A" Bars	bent 9'-10" overall. Lapped 6" and wired.
"B" "	" 13'-11" " " 8" " "
"C" "	straight 13'-11" long. Top and bottom.
	Ends are 1 1/2" from edge of slab.
	Each 10'x40' slab contains 25 "A" Bars - 18 "B" Bars and 12 "C" Bars made up in 3 sections. Each section to be securely wired before placing.
	Each 10'x40' slab contains a minimum of 42 cement blocks and 42 blocks and spacers to support steel. See Detail.
	Total weight of steel including dowels 10.74 lb per square yard.





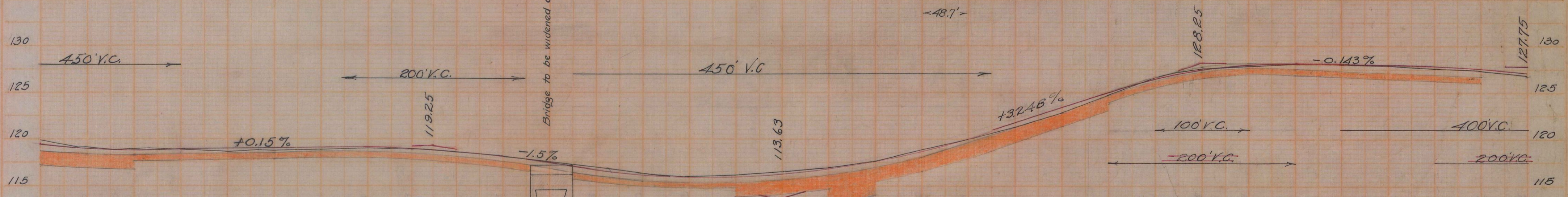




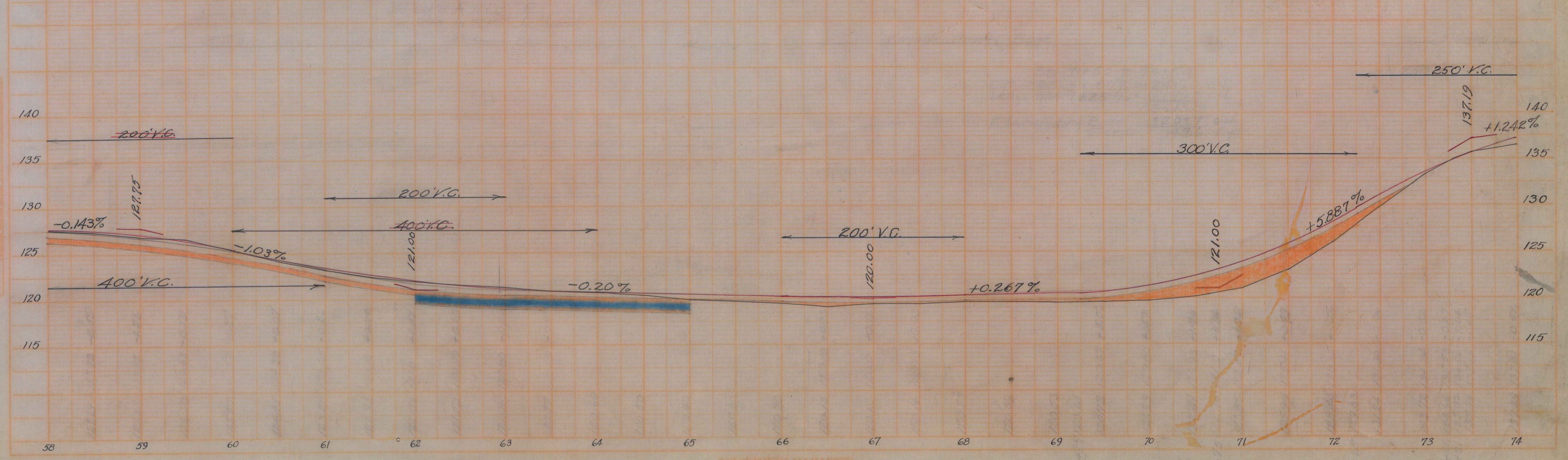
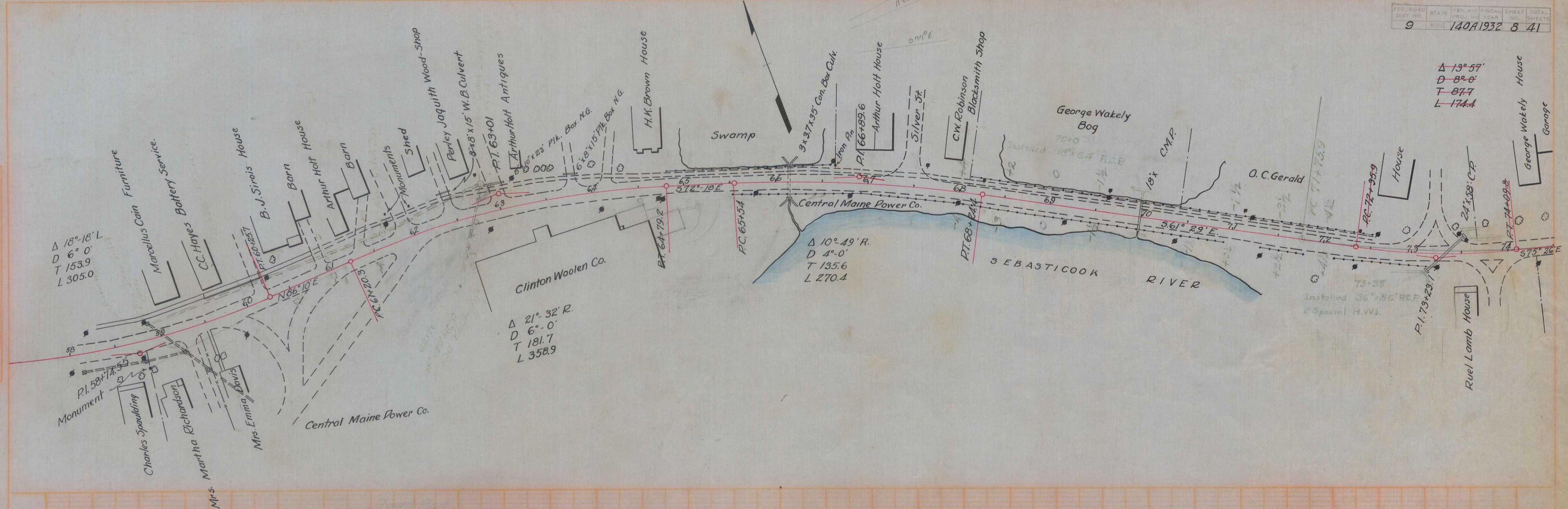
Note:
Widening and Super-elevation
from Sta 46+50 to Sta 53+0
is not standard

Left
Bridge to be widened on right by State.

SPECIAL WIDENING AT BRIDGE

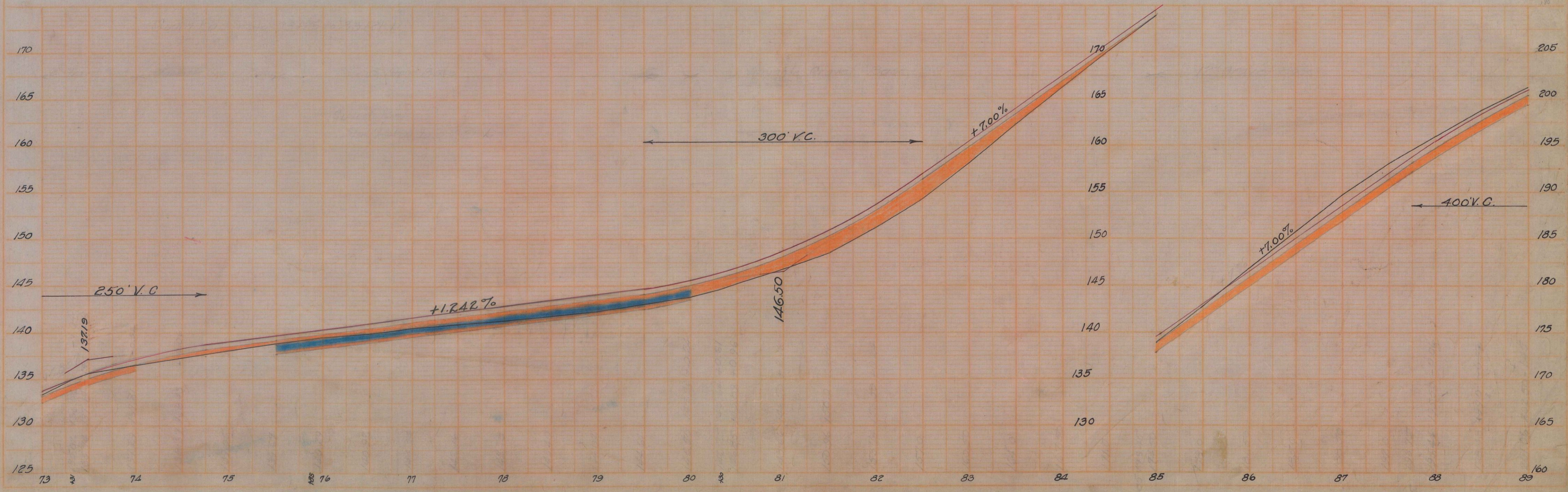
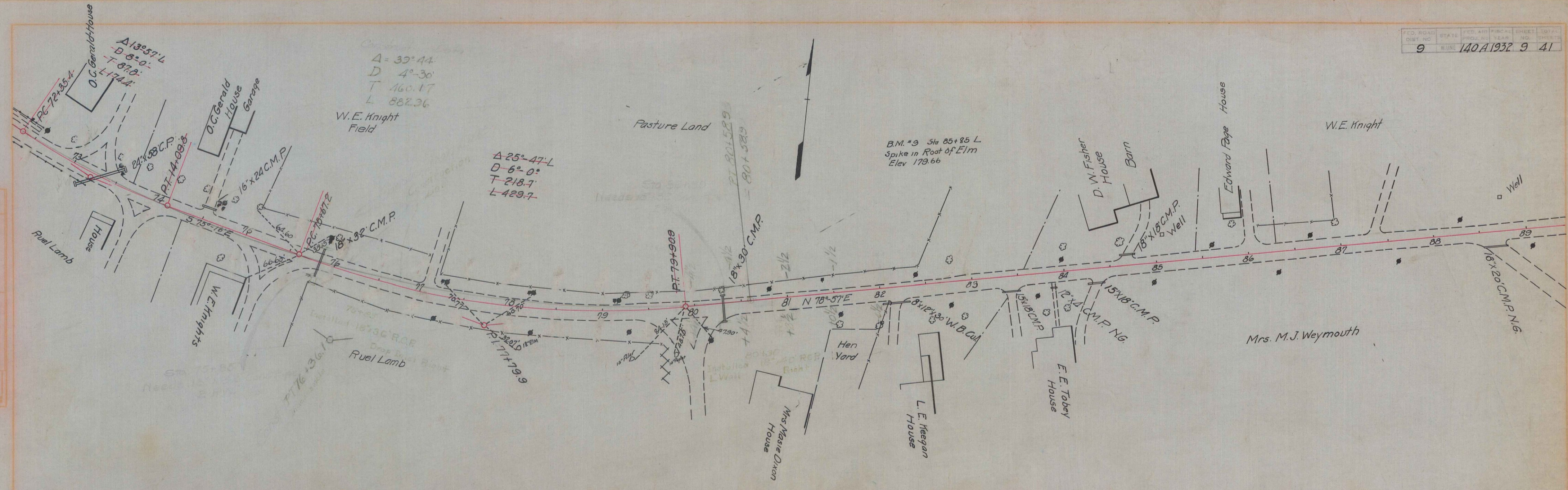


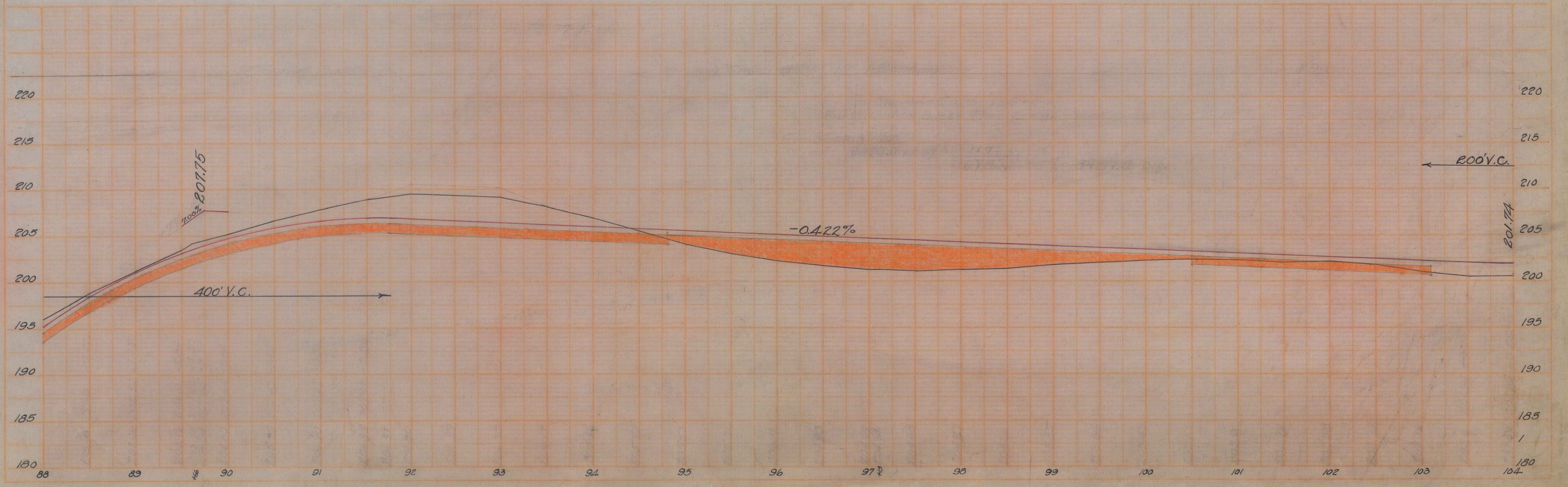
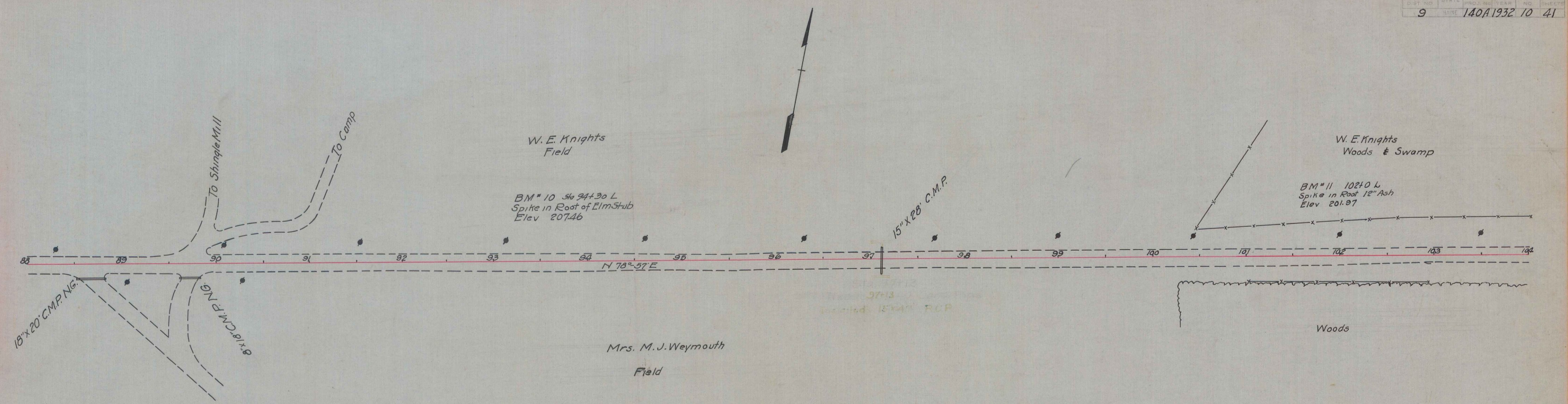
$\Delta 13^{\circ} 57'$
 $D 8^{\circ} 0'$
 $T 87.7$
 $L 174.4$

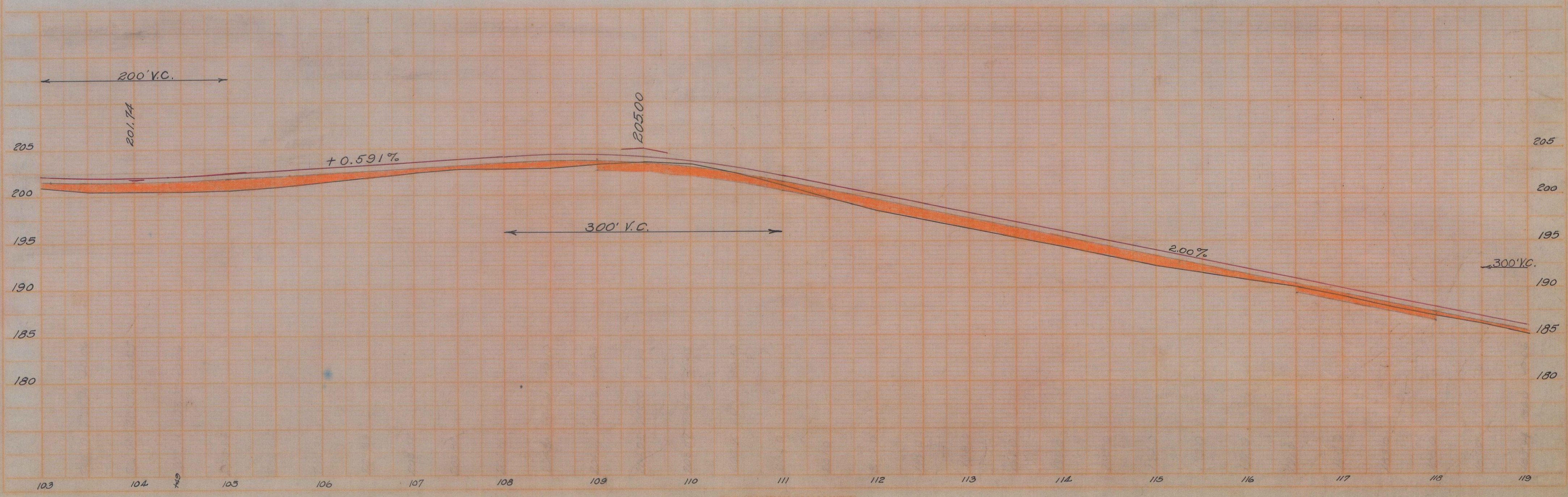
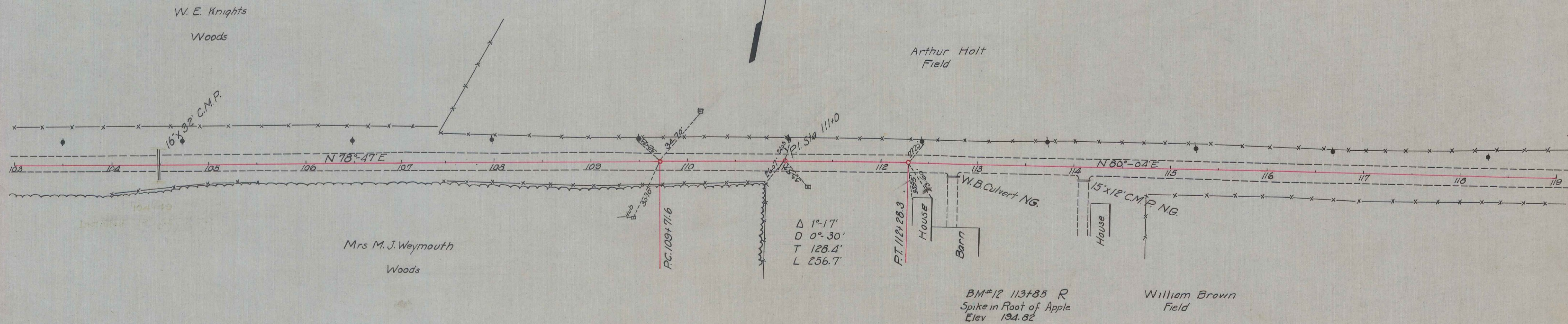


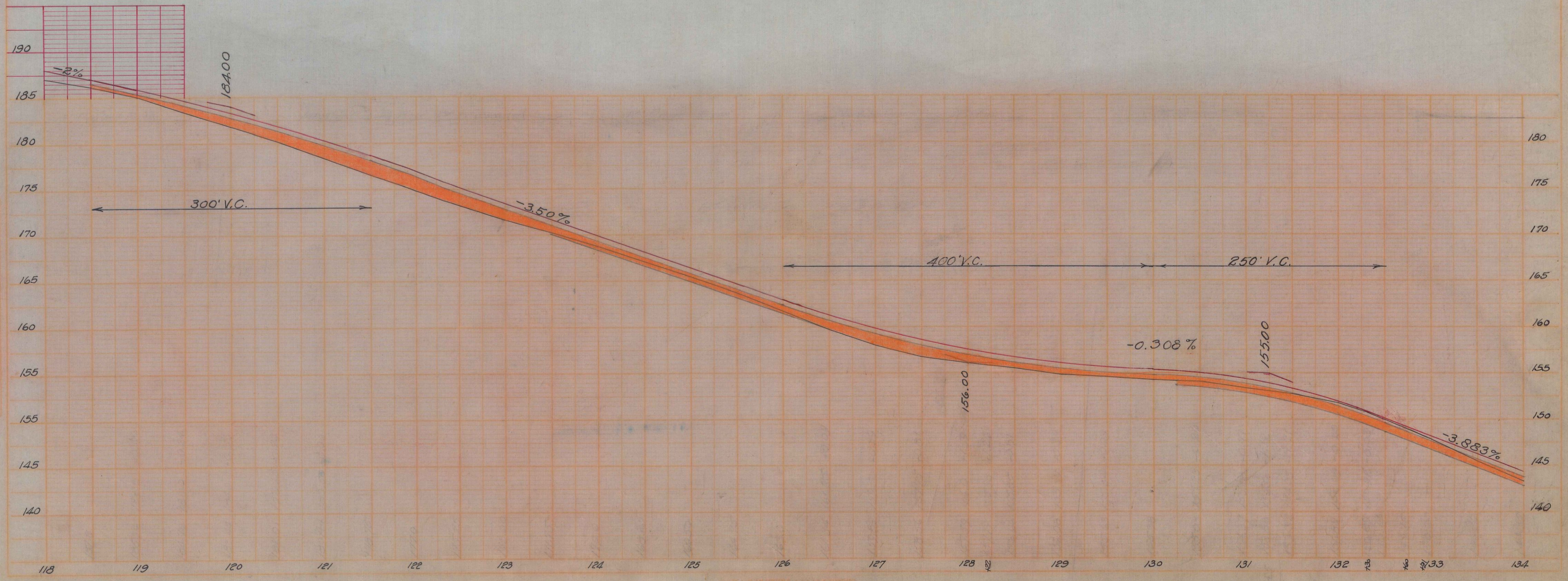
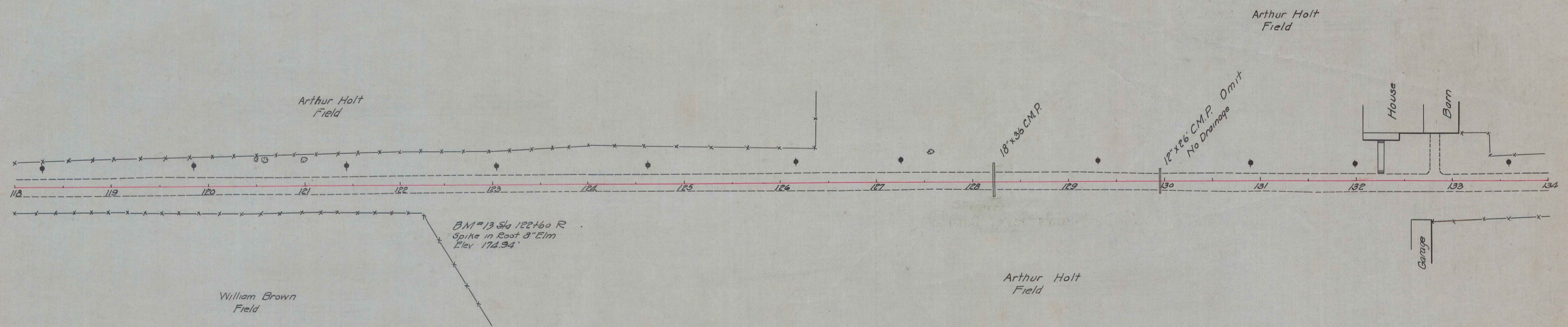
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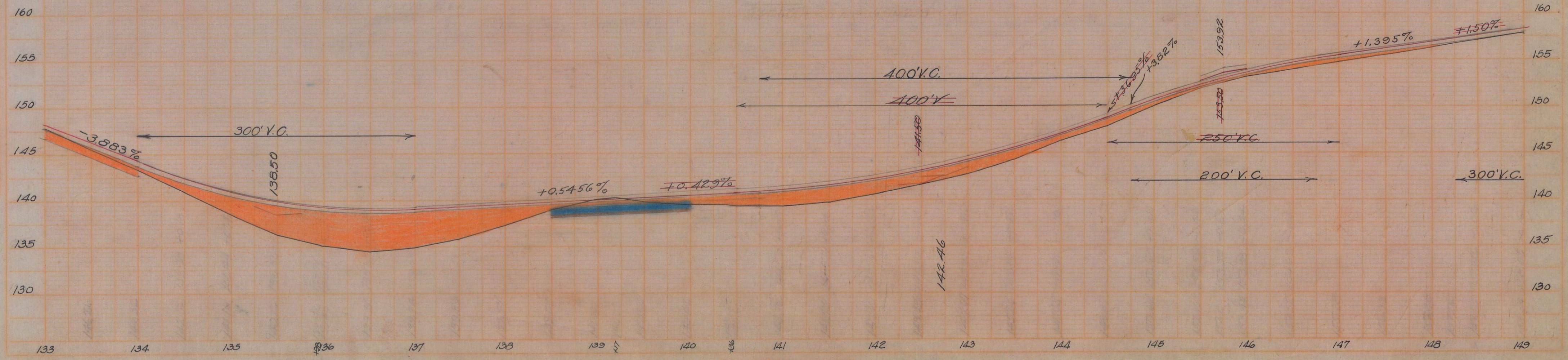
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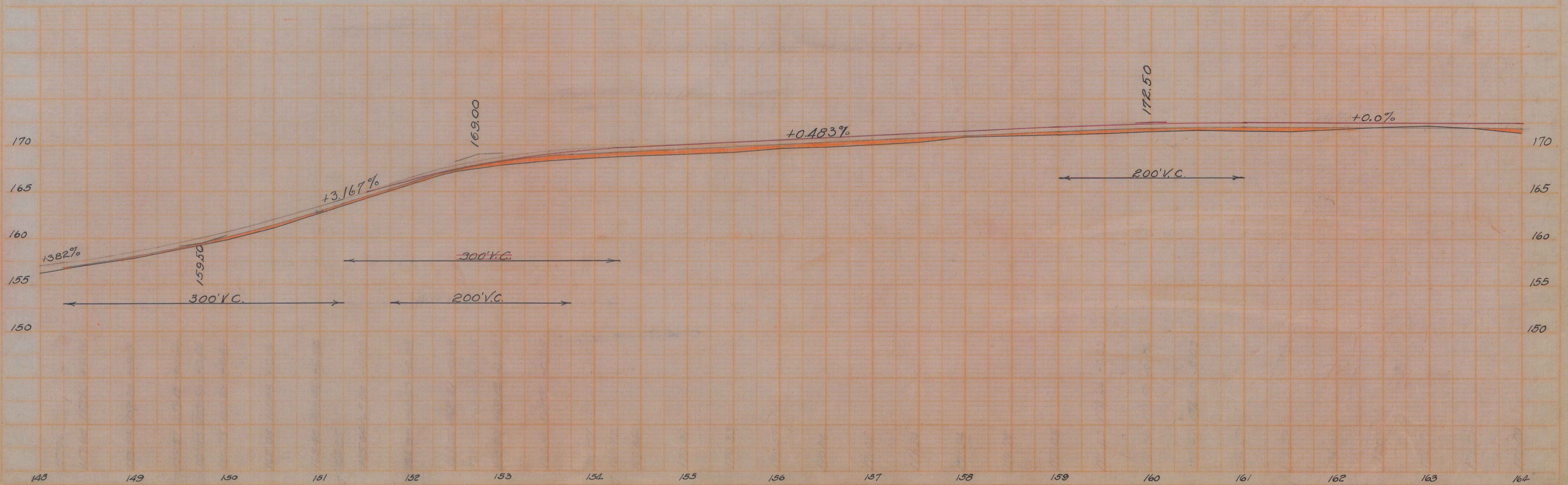
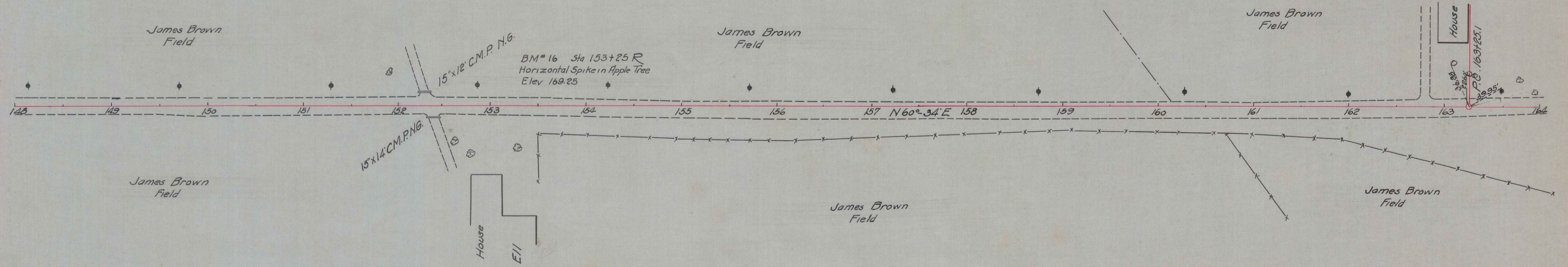


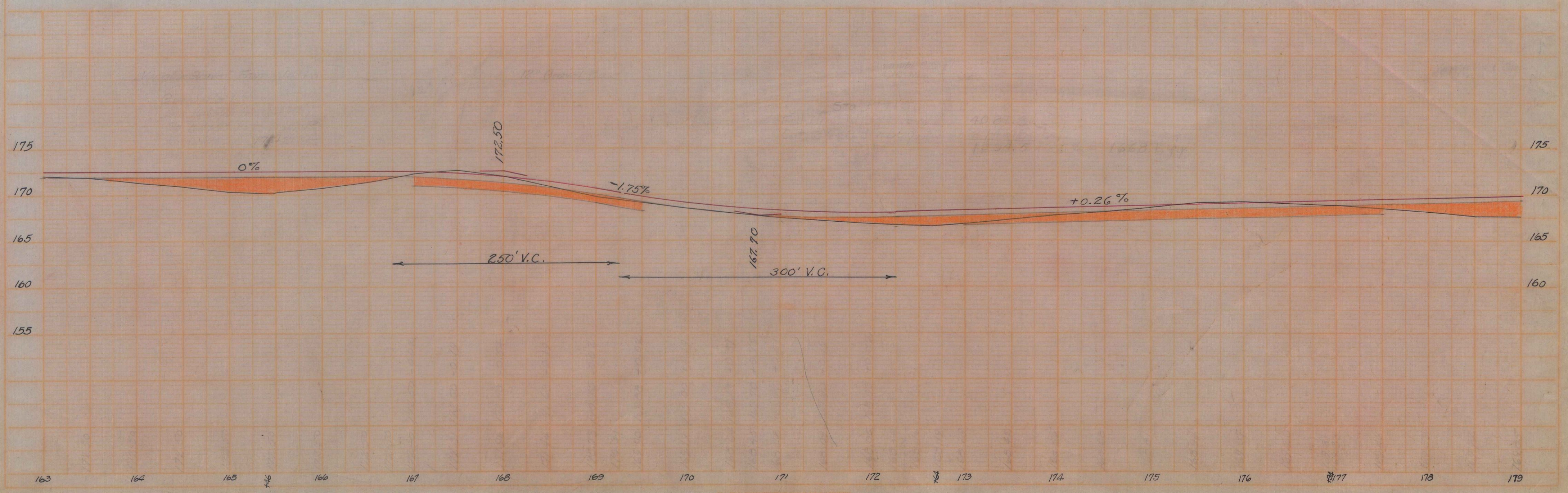
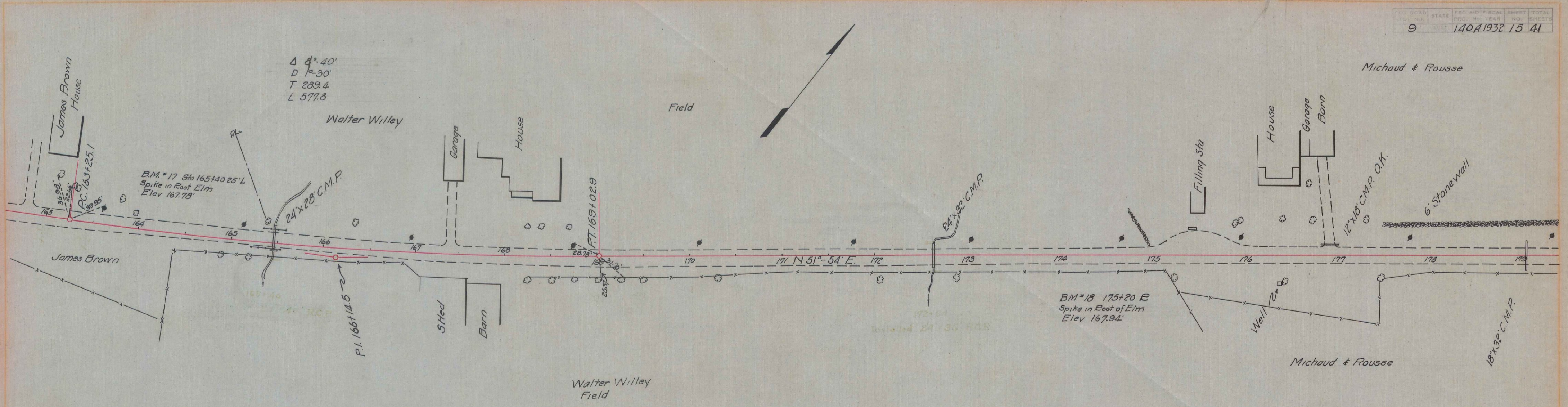


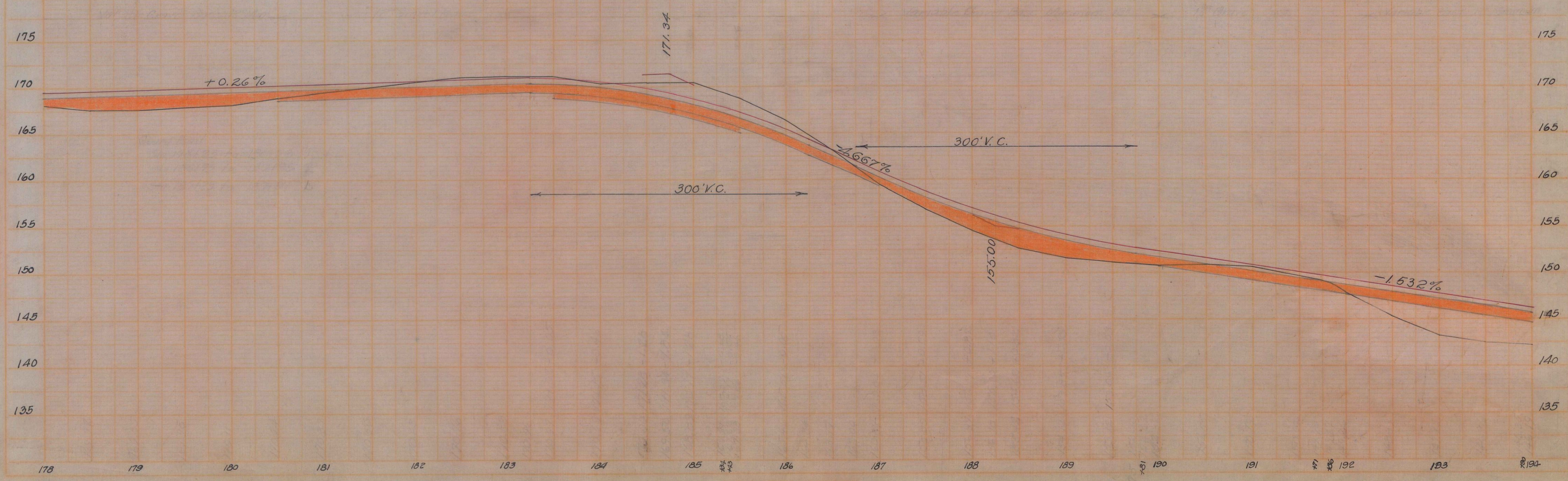
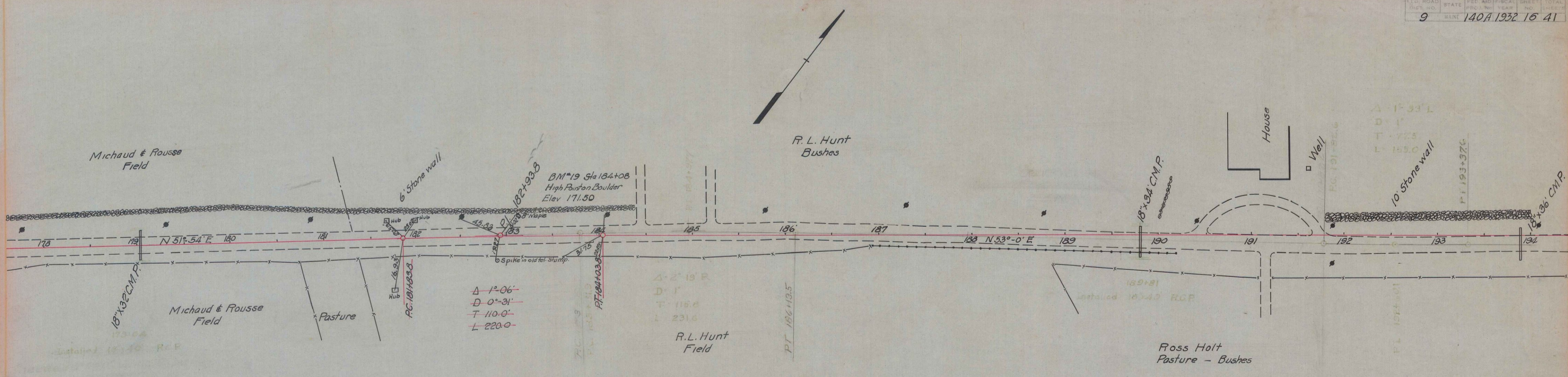


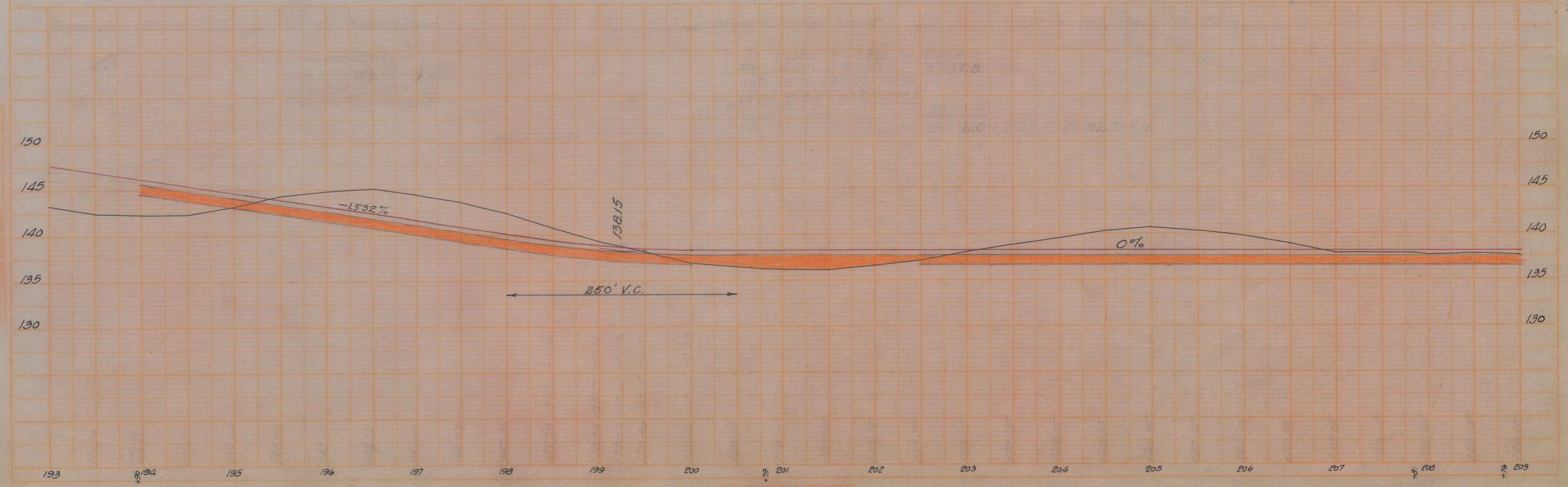
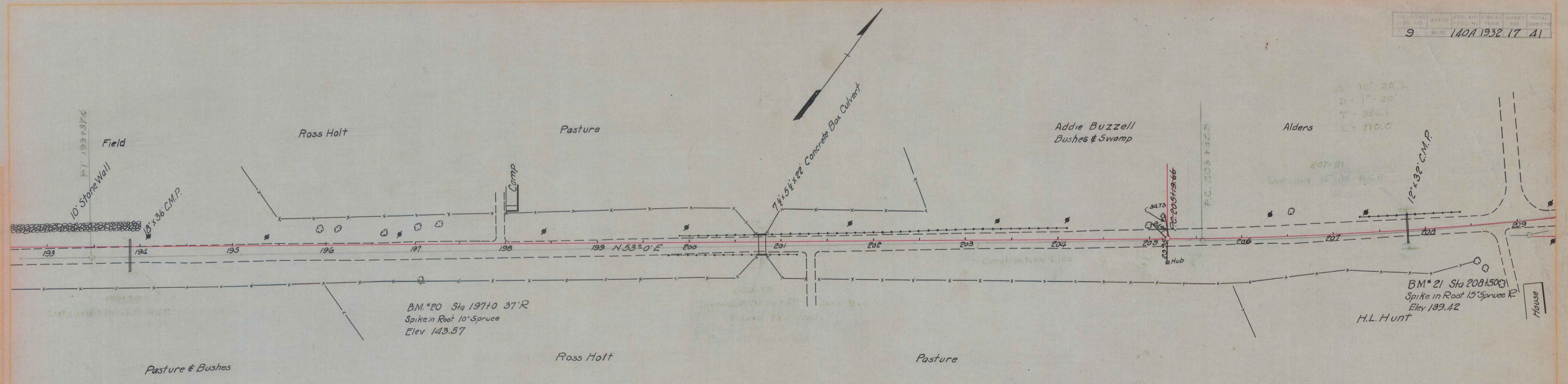


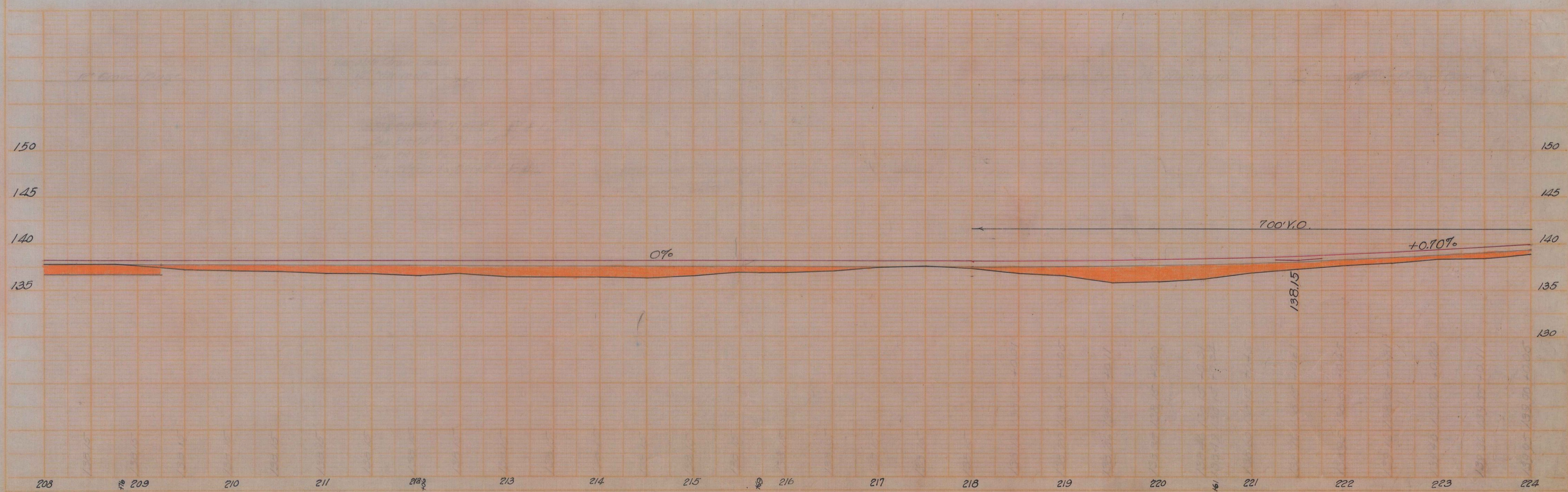
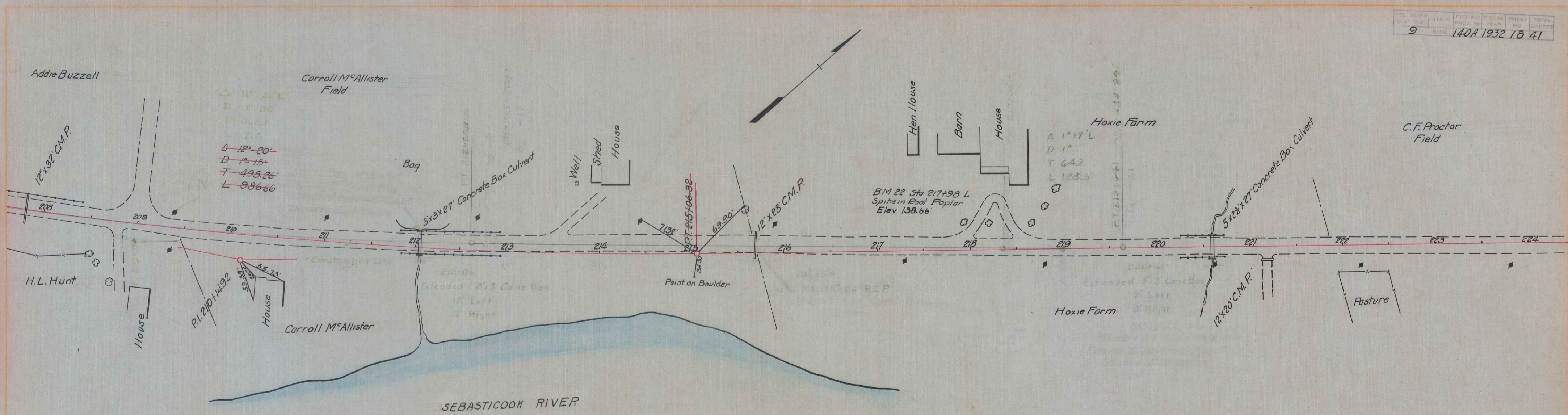












12x12 C.M.P.	208
3x3x27 Concrete Box Culvert	212
12x28 C.M.P.	216
5x2x27 Concrete Box Culvert	221
12x20 C.M.P.	222

135	208
138.15	221
140	218
145	215
150	212

