

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
9	MAINE	294-E(1)		1	13

WELLS

STATE OF MAINE
STATE HIGHWAY COMMISSION

PLAN AND PROFILE
STATE HIGHWAY #A-2
(HIGH PINE CROSSING)
WELLS

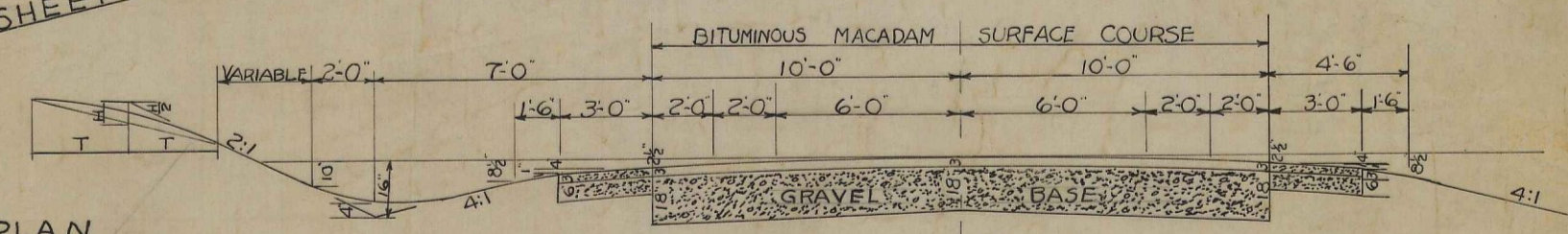
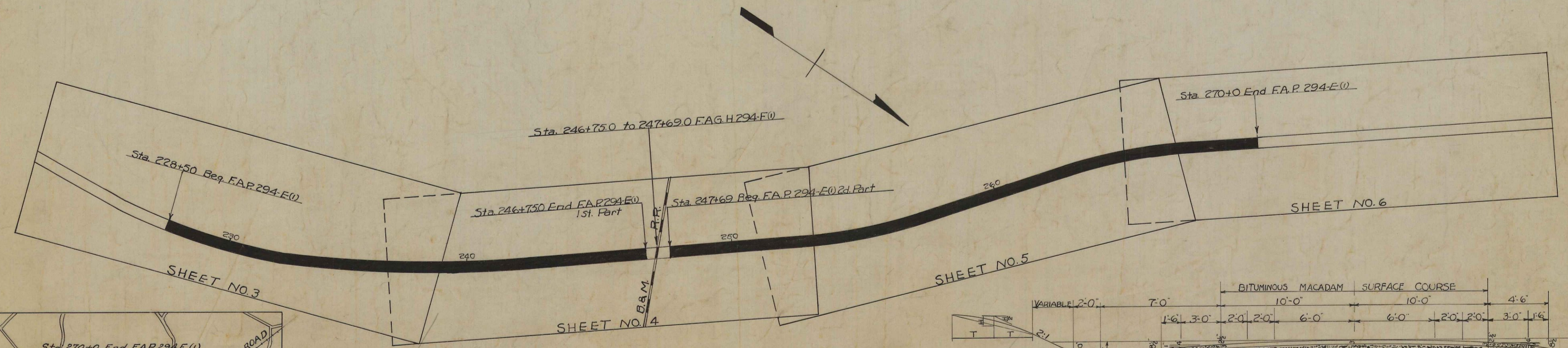
YORK COUNTY
FEDERAL AID PROJECT NO. 294-E (1)

Reel 21

CONVENTIONAL SIGNS	
STATE OR NATIONAL LINE	-----
COUNTY LINE	-----
TOWN LINE	-----
UNFENCED PROPERTY	-----
FENCE	-----
RIGHT OF WAY LINE	-----
TRAVELED WAY	-----
RAILROAD	-----
RETAINING WALL	-----
SURVEY LINE	-----
CULVERT	-----
DROP INLET	-----
TROLLEY POLE	-----
POWER POLE	-----
TEL. POLE	-----
MARSH	-----
TREES	-----
STONE WALL	-----

TOTAL LENGTH 0.768 MILES
 SCALES { PLAN 1 IN. = 50 FT.
 { PROFILE { HOR. 1 IN. = 50 FT.
 { VER. 1 IN. = 5 FT.
 { CROSS SECTIONS 1 IN. = 5 FT.

INDEX OF SHEETS		
SHEET No. 1	TITLE PAGE	STA. 228+50 - 246+75
SHEET No. 2	TYPICAL SECTIONS	247+69 - 270+0
SHEET No. 3-6	PLAN AND PROFILE	STA. 228+50 - 246+75
SHEET No. 7-13	CROSS-SECTIONS	STA. 247+69 - 270+0
SHEET No.	BRIDGES	STA. 228+50 - 246+75
SHEET No.	SPECIAL DETAILS	STA. 247+69 - 270+0



A PORTION OF YORK COUNTY
Approx. Scale: 1 in. = 1 Mi.

NOTE: - ALL WORK CONTEMPLATED BY THIS CONTRACT SHALL BE GOVERNED BY AND IN CONFORMITY WITH THE SPECIFICATIONS ACCOMPANYING THESE PLANS.

APPROVED:
 MAINE STATE HIGHWAY COMMISSION
Chairman
Chief Engineer

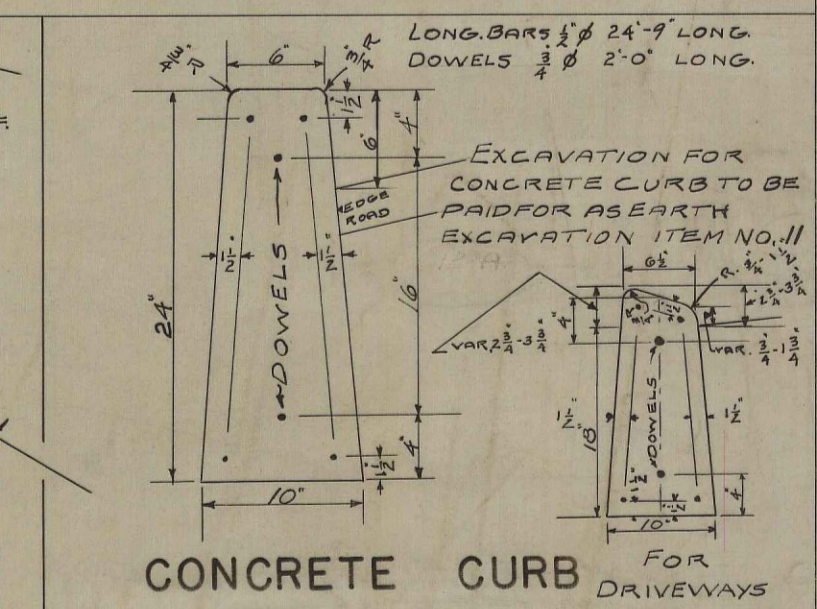
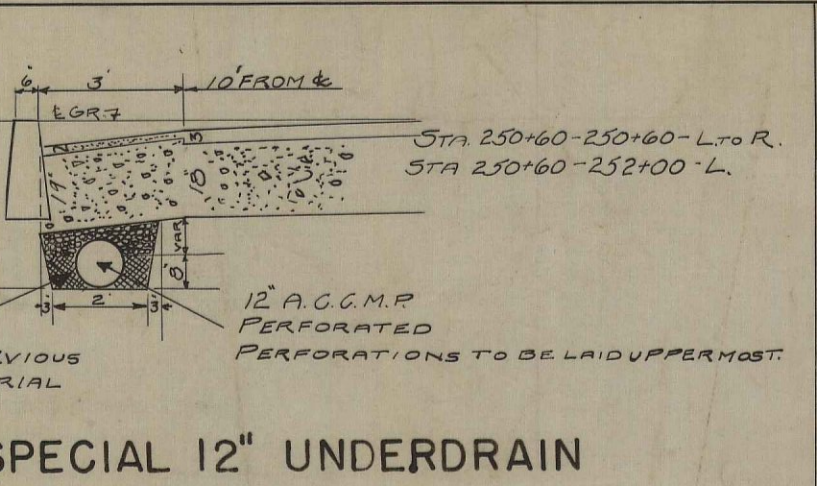
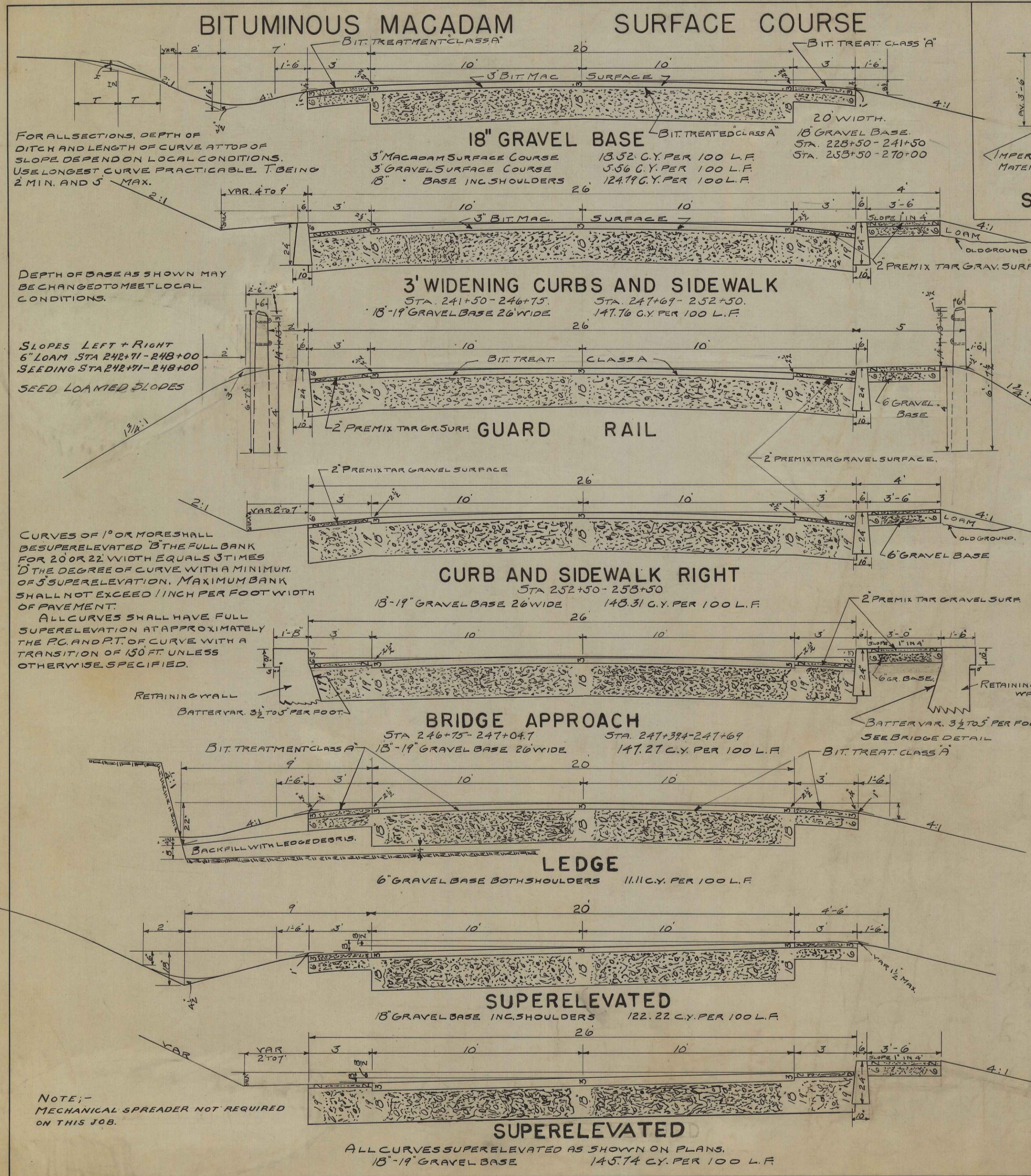
RECOMMENDED TO THE DISTRICT ENGINEER FOR APPROVAL

SENIOR HIGHWAY ENGINEER
 PUBLIC ROADS ADMINISTRATION
 FEDERAL WORKS AGENCY
 RECOMMENDED FOR APPROVAL

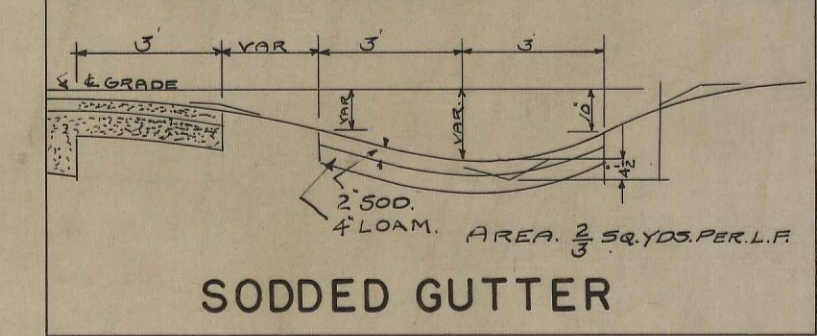
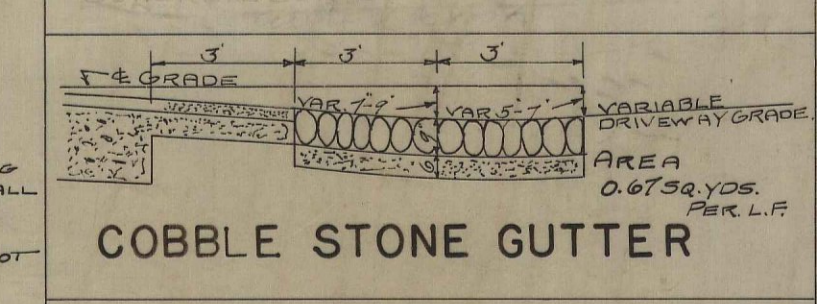
DISTRICT ENGINEER
 PUBLIC ROADS ADMINISTRATION
 FEDERAL WORKS AGENCY

APPROVED
 COMMISSIONER
 PUBLIC ROADS ADMINISTRATION
 FEDERAL WORKS AGENCY

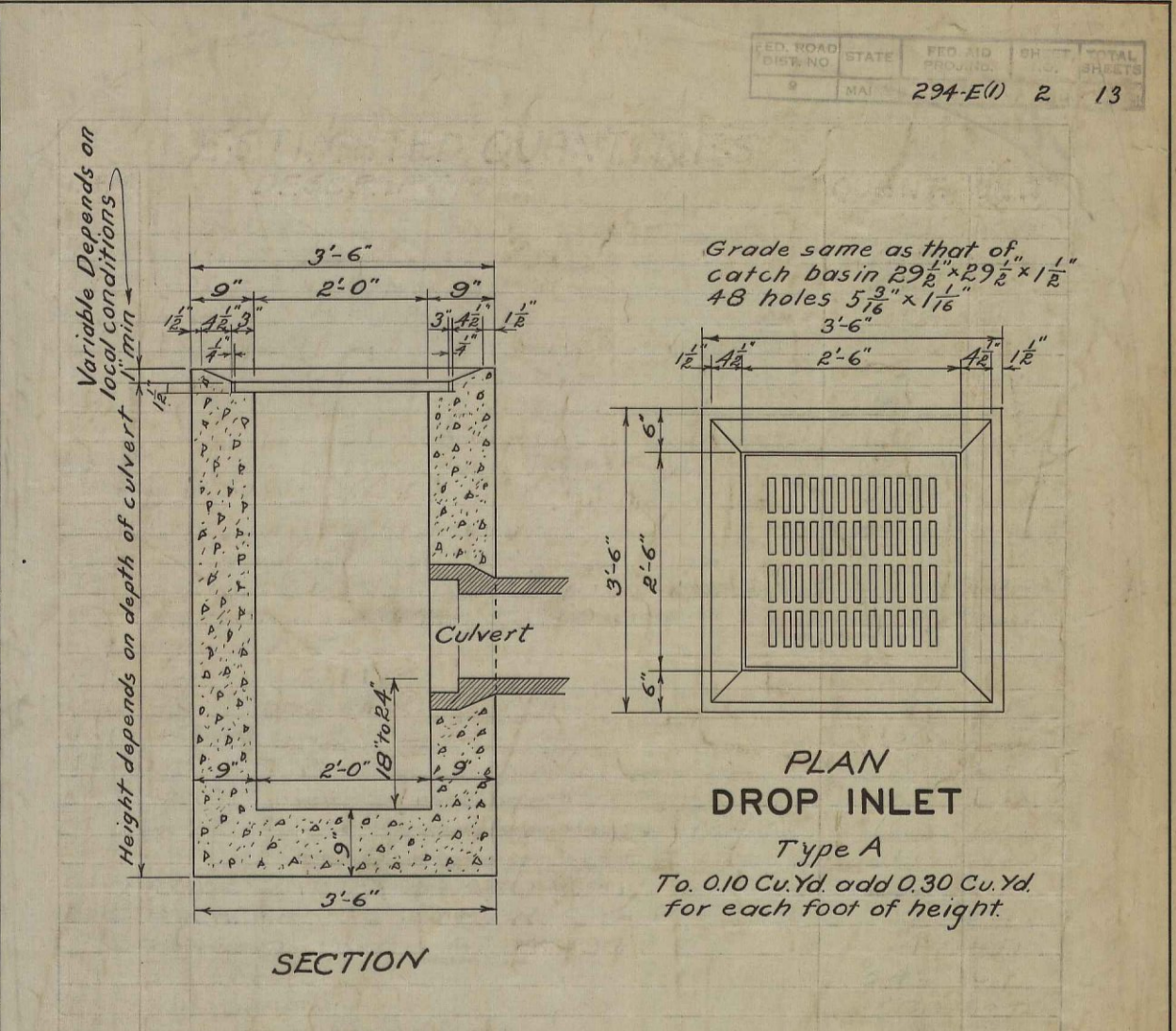
AA-29



CONCRETE CURB TO BE BUILT REINFORCED AS SHOWN. EXPANSION AND CONSTRUCTION JOINTS TO BE LOCATED ALTERNATELY AT 25 FT. INTERVALS WITH 3" DOWELS 2'-0" LONG HALF INCISED TO ALLOW FOR EXPANSION. EXPANSION MATERIAL SHALL BE EITHER PREMOULDED CORK OR PREMOULDED CORK RUBBER AND SHALL BE 1/2" THICK. CURB TO BE PAID FOR UNDER ITEM #55 OF THE PROPOSAL.



NOTE.
 ALL GRAVEL BASE IN 20 FT. ROADWAY TO BE BIT. TREATED CLASS A
 ALL 3" WIDENING LEFT AND RIGHT TO BE SURFACED WITH A 2" PREMIX TAR GRAVEL SURFACE.
 SIDEWALK 3'-6" WITH A 6" GRAVEL BASE AND A 2" PREMIX TAR GRAVEL SURFACE.



CEMENT RUBBLE MASONRY END WALLS FOR DRIVEWAY CULVERTS

One end wall

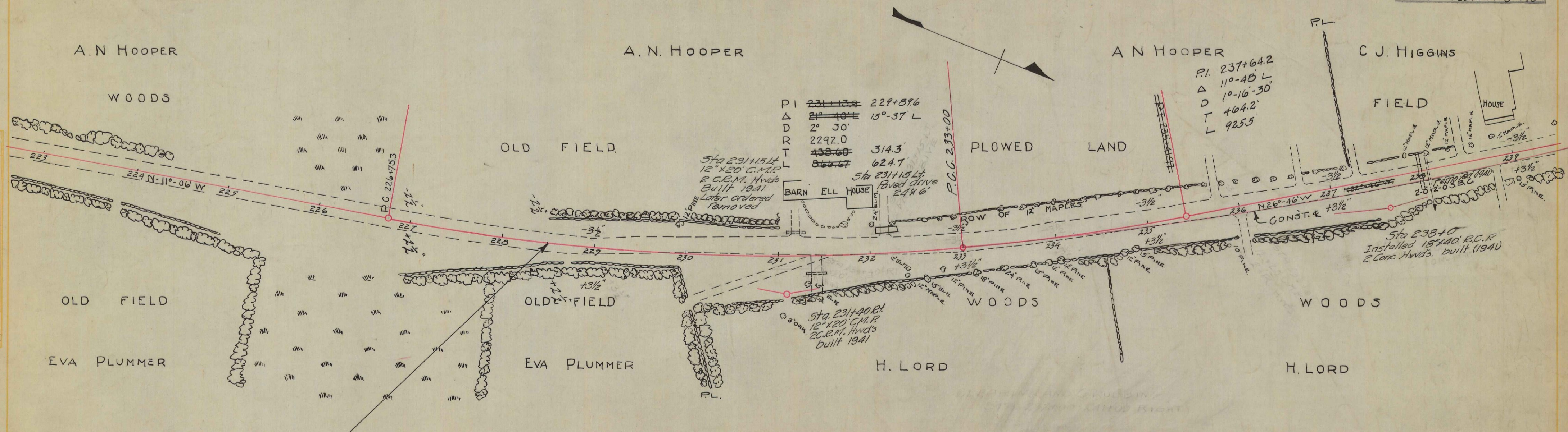
D	L	H	B	Rubble Masonry Cu. Yds.
12"	3'-0"	2'-3"	1'-6"	0.44
15"	3'-3"	2'-6"	1'-6"	0.50
18"	3'-6"	2'-9"	1'-7"	0.58
24"	4'-0"	3'-3"	1'-8"	0.74

End wall flared to fit road shoulder.

NOTE.
 WHEN FEASIBLE, MONUMENTS SHALL BE SET IN RIGHT OF WAY LINES OF RIGHT OF WAY ACQUIRED BY THE STATE OPPOSITE THE BEG. AND END OF CURVES, AT ANGLE POINTS, AND AT SUITABLE INTERMEDIATE LOCATIONS DESIGNATED BY THE ENGINEER SO THAT A MONUMENT ON EACH SIDE MAY BE OBSERVED FROM EACH MONUMENT.

CULVERTS			CULVERTS.		
STATION	PIPE	REMARKS	STATION	PIPE	REMARKS
231+15	12" X 20" C.M.P.	DRIVE L	252+00	DROP INLET	L
231+40	12" X 20" C.M.P.	" R	251+30	"	L
236+00	12" X 20" C.M.P.	"	250+00	"	L & R
230+07	15" X 44" R.C.P.	"	250+60	"	25' R
239+60	15" X 45" R.C.P.	"			
242+71	15" X 50" R.C.P.	D.I. L & R TYPE A			
242+80	12" X 20" C.M.P.	DRIVE R			
249+00	15" X 75" A.C.C.M.P.	25' R SEWER OUTLET			
250+00	12" X 9" A.C.C.M.P.	LEFT			
250+60	12" X 22" A.C.C.M.P.	SPECIAL 12" UNDERDRAIN TYPE B			
250+60-252+00	12" X 140" "	"			
261+00-R	12" X 20" C.M.P.	DRIVE R			
249+60-250+60	12" X 100" A.C.C.M.P.	STORM SEWER - R			
261+04	15" X 40" R.C.P.	SEWER			

NOTE:
 MECHANICAL SPREADER NOT REQUIRED ON THIS JOB.

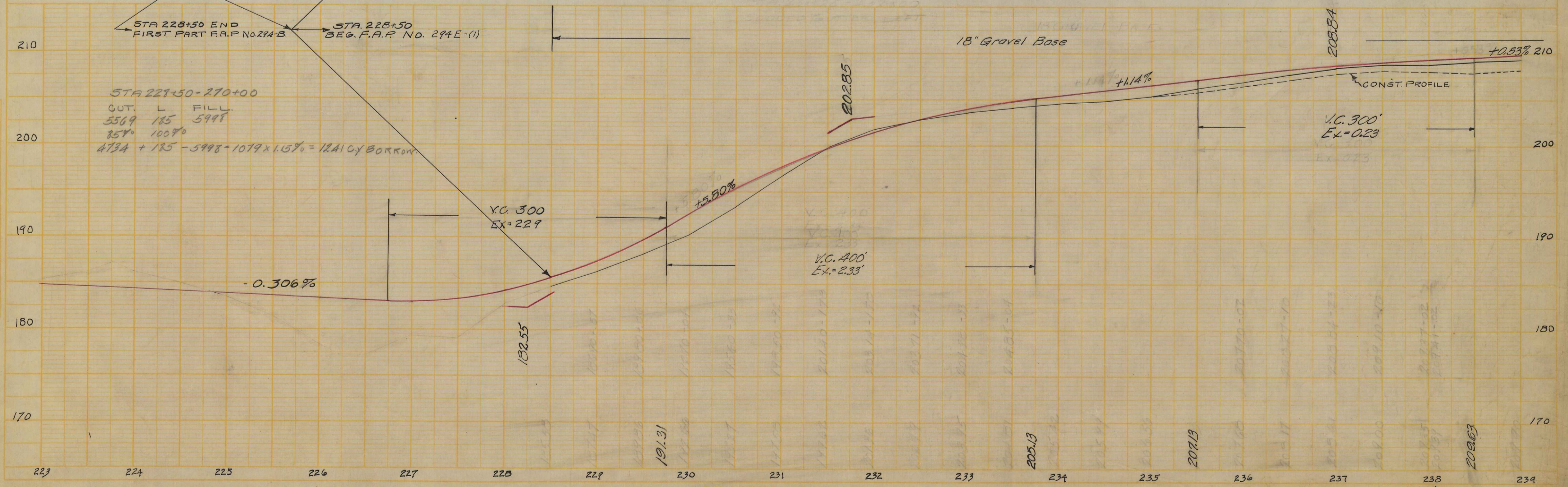


PI	231+15.4	229+89.6
Δ	2° 30' E	15° 37' L
D	2292.0	314.3
T	438.60	624.7
L	866.67	

P.I. 237+64.2
 Δ 110-48 L
 D 10-16-30
 T 464.2
 L 925.5

STA 228+50 END FIRST PART F.A.P. NO. 294-B
 STA 228+50 BEG. F.A.P. NO. 294-E-(1)

STA 229+50-270+00
 CUT. L. FILL
 5569 185 5991
 154 100 90
 $4734 + 185 - 5998 - 1079 \times 1.15\% = 1221 \text{ CY BORROW}$

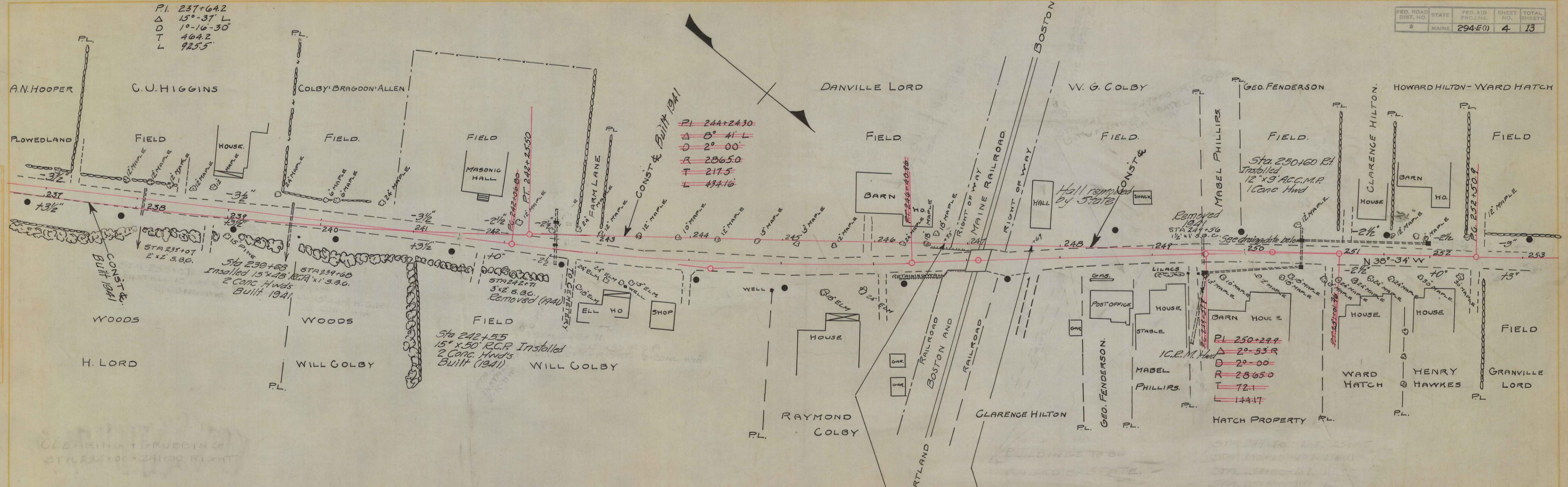


V.C. 300
 Ex = 229

V.C. 400
 Ex = 233

V.C. 300
 Ex = 0.23

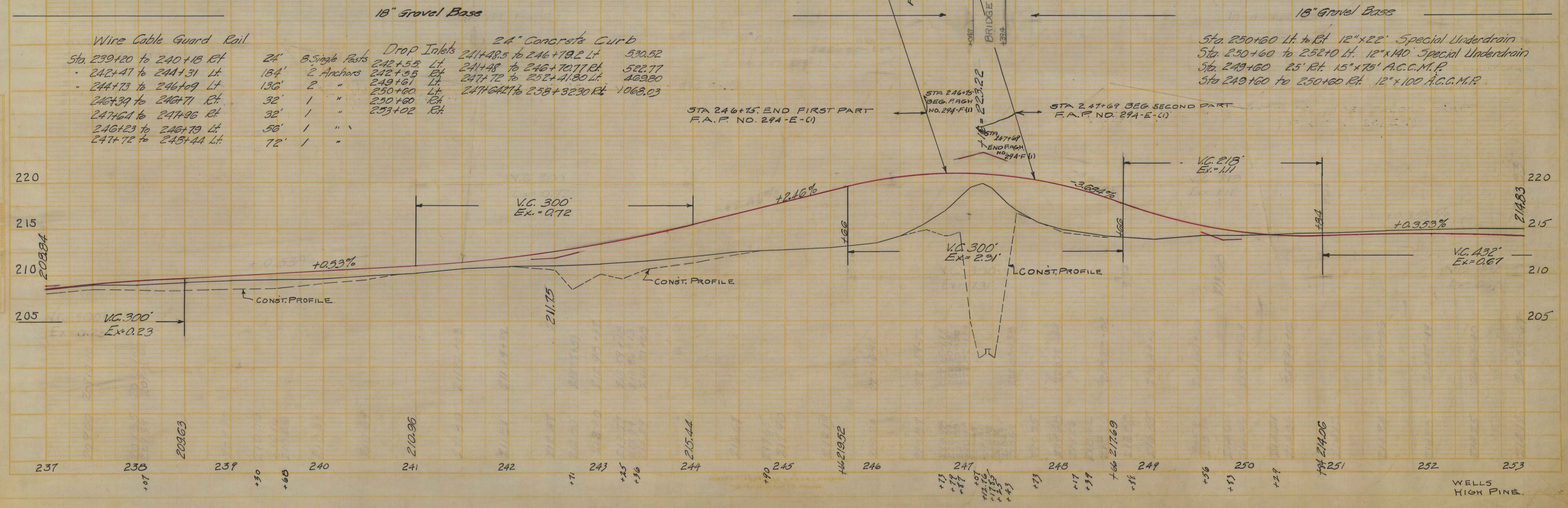
WELLS
 HIGH PINE



P.I. 237+64.2
 Δ 15°-37' L
 D T 10'-16'-30"
 L 464.2
 L 925.5'

P.I. 244+24.30
 Δ 0°-41' L
 D 2°-00"
 R 2065.0
 T 217.5
 L 434.16'

P.I. 250+24.4
 Δ 2°-53' R
 D 2°-00"
 R 2865.0
 T 72.1
 L 144.17'



Wire Cable Guard Rail

Sta. 239+20 to 240+15 Rt.	24'	8 Single Posts
242+47 to 244+31 Lt.	184'	2 Anchors
244+73 to 246+09 Lt.	136'	2 "
246+39 to 248+71 Rt.	32'	1 "
247+64 to 247+96 Rt.	32'	1 "
246+23 to 246+79 Lt.	56'	1 "
247+72 to 248+44 Lt.	72'	1 "

Drop Inlets

242+55 Lt.	241+48.5 to 246+79.2 Lt.	530.52
242+55 Rt.	241+48 to 246+70.77 Rt.	522.77
249+61 Lt.	247+72 to 252+41.80 Lt.	469.80
250+60 Rt.	247+62.16 to 258+32.30 Rt.	1068.03
253+02 Rt.		

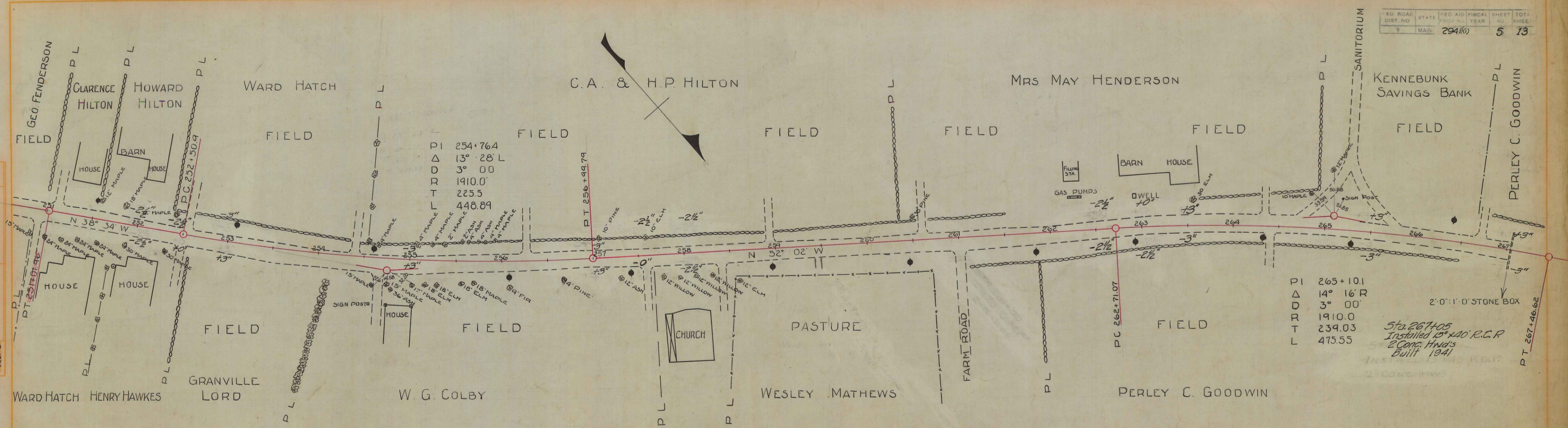
Sta. 250+60 Lt. to Rt. 12"x22" Special Underdrain
 Sta. 250+60 to 252+10 Lt. 12"x140" Special Underdrain
 Sta. 249+60 25' Rt. 15"x75" A.C.C.M.P.
 Sta. 249+60 to 250+60 Rt. 12"x100" A.C.C.M.P.

STA 246+75 END FIRST PART F.A.P. NO. 294-E-(1)
 STA 247+69 BEG. F.A.P. NO. 294-F(1)
 STA 247+69 END F.A.P. NO. 294-F(1)
 STA 247+69 BEG. SECOND PART F.A.P. NO. 294-E-(1)

WELLS HIGH PINE.

PLAN
 DATE: 12-31-34
 DRAWN BY: E.S. METCALF
 CHECKED BY: LIBBY 1-35
 PROJECT: A-32-3

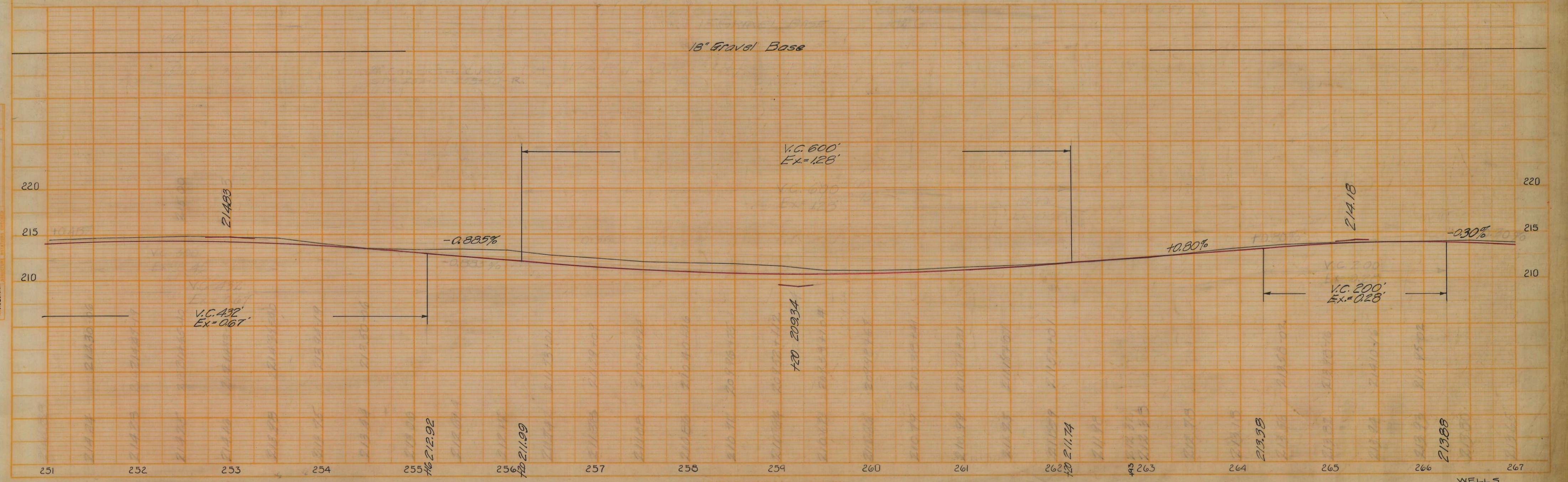
PROFILE
 DATE: 12-31-34
 DRAWN BY: E.S. METCALF
 CHECKED BY: METCALF
 PROJECT: A-32-4

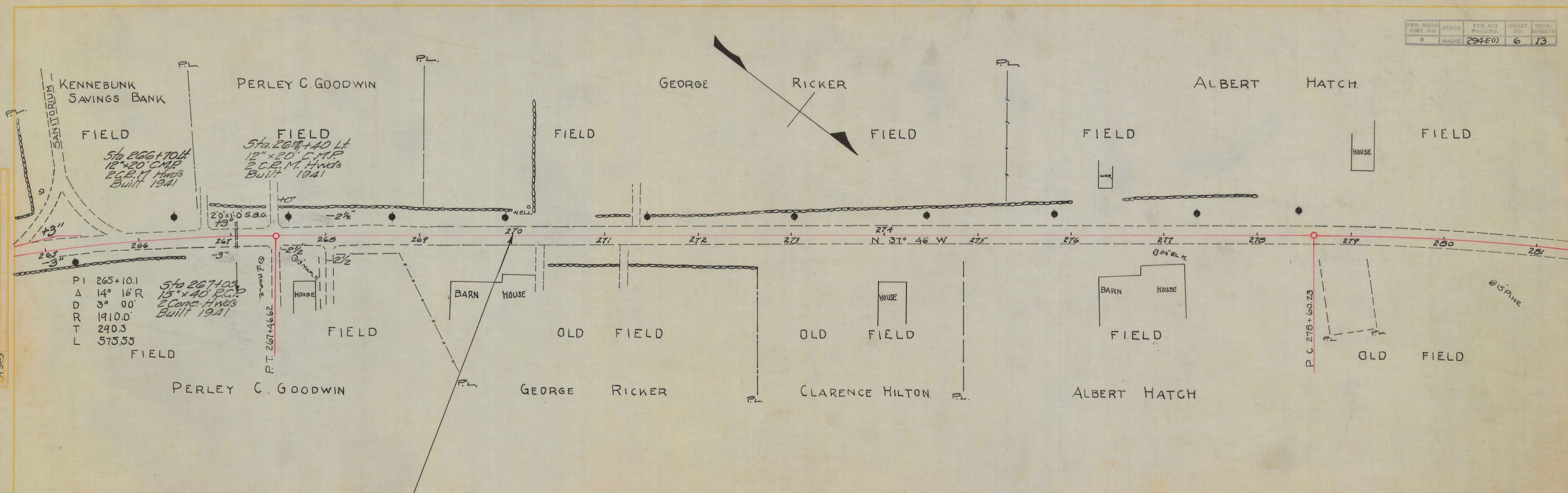


PI 254+76.4
 Δ 13° 28' L
 O 3° 00'
 R 1910.0'
 T 2255
 L 448.89

PI 265+10.1
 Δ 14° 16' R
 O 3° 00'
 R 1910.0'
 T 234.03
 L 473.55

Sta. 267+05
 Installed 12" x 40" R.C.P.
 2' CONC. HAND'S
 BUILT 1941

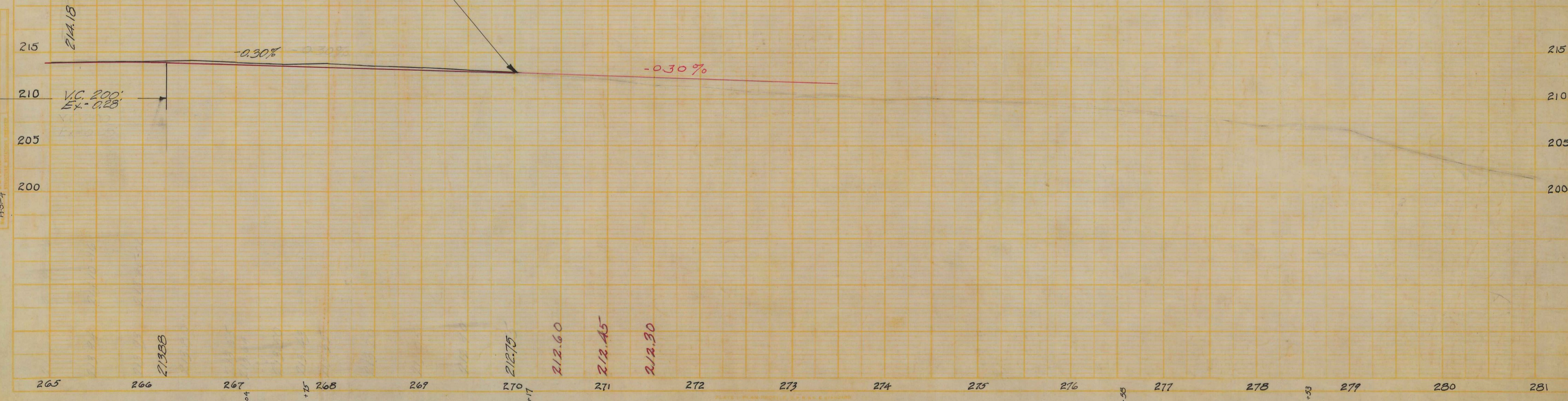




12-34
 1-35
 E.S. METCALF
 FOSTER
 A-353

PI 265+10.1
 Δ 14° 16' R
 D 3° 00'
 RT 1910.0'
 L 575.55
 FIELD
 Sta. 267+10.1
 15' x 40' C.M.P.
 2 Cons. Hwys
 Built 1941

STA. 270+00 END F.A.P. NO. 294-E-(1)
 15" Gravel Base
 STA. 270+00 BEG. SECOND PART F.A.P. NO. 294-B



12-34
 1-35
 E.S. METCALF
 METCALF-FOSTER
 A-354